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CITY SECRETARY
DALLAS, TEXAS

TRANSPORTATION & TRINITY RIVER PROJECT COMMITTEE
DALLAS CITY COUNCIL COMMITTEE AGENDA

MONDAY, FEBRUARY 27, 2017
CITY HALL
COUNCIL BRIEFING ROOM, 6ES
1500 MARILLA
DALLAS, TEXAS 75201
1:00 P.M. – 2:30 P.M.

Chair, Councilmember Lee M. Kleinman
Vice-Chair, Deputy Mayor Pro Tem Erik Wilson
Councilmember Sandy Greyson
Mayor Pro Tem Monica R. Alonzo
Councilmember Adam Medrano
Councilmember Casey Thomas, II

Call to Order

1. Approval of Minutes

BRIEFINGS

2. Urban Transit Design Guidelines Peer Chacko, Chief Planning Officer
and Director, Planning and Urban Design
3. Advanced Traffic Management System Project Update Auro Majumdar, Assistant Director
Mobility and Street Services
4. **MONTHLY INTERAGENCY TRANSPORTATION REPORT (For Informational Purposes only)**
5. **UPCOMING AGENDA ITEMS**

March 22, 2017

- A. Draft Agenda Item: Authorize (1) the Aviation Department to provide initial funding for maintenance costs for the Routine Annual Maintenance Program (RAMP) at Dallas Executive Airport in an amount not to exceed \$100,000; (2) the Aviation Department to apply for and accept grant funds from the Texas Department of Transportation Aviation Division in an amount not to exceed \$50,000 for eligible costs related to RAMP as part of a 50% conditional grant match agreement; and (3) execution of the grant agreement - Total not to exceed \$100,000 (AVI)
- B. Draft Agenda Item: Authorize Supplemental Agreement No. 4 to the professional services contract with Garver, LLC for design, planning, engineering, and construction document services for the Love Field Pavement Rehabilitation and Reconstruction Project, Taxiway Bravo Phase II, and Taxiway Mike Phase II at Dallas Love Field - Not to exceed \$1,737,770, from \$3,456,534 to \$5,194,304 (AVI)
- C. Draft Agenda Item: Authorize an amendment to an Interlocal Agreement between the City of Dallas, City of Grapevine and the Dallas County Park Cities Municipal Utility District, to extend the term of the contract by ten-years, with four ten-year extension options, for continuation of the reimbursement to the City of Grapevine for Dallas' cost share for the administration of the Lake Grapevine Reservoir Accounting Plan - Not to exceed \$60,000 (DWU)
- D. Draft Agenda Item: Authorize (1) an Interlocal Agreement with the Texas Department of Transportation to provide cost reimbursement for material and labor costs incurred by the City for intersection and traffic signal improvements at the following five off-system locations: Marsh Lane and Rosemeade Parkway, Bruton Road and Second Avenue, Forest Lane and Audelia Road, Plano Road and Miller Road and Royal Lane and Skillman Road; (2) the receipt and deposit of funds from the Texas Department of Transportation in the amount of \$101,280; and (3) the establishment of appropriations in the amount of \$101,280 in the Texas Department of Transportation Grant Fund - Not to exceed \$101,280 (MSS)

A quorum of the City Council may attend this Council Committee meeting

- E. Draft Agenda Item: Authorize payment to Dallas Area Rapid Transit for operation and maintenance costs for the Dallas Streetcar System (Union Station to Bishop Arts District) for Fiscal Year 2017 - Not to exceed \$975,000 (MSS)
- F. Draft Agenda Item: Authorize a professional services contract with Huitt-Zollars, Inc. for the engineering design of complete street improvements for Knox Street from Katy Trail to US 75 - Not to exceed \$307,935 (MSS)
- G. Draft Agenda Item: Authorize a construction contract with Ark Contracting Services, LLC, lowest responsible bidder of four, for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2040 and 12-2045, and paving improvements for Merrifield Road Dead End Turnaround Project (list attached) - Not to exceed \$4,174,616 (MSS)
- H. Draft Agenda Item: A public hearing to receive comments to amend the City of Dallas Central Business District Streets and Vehicular Circulation Plan to delete Live Oak Street from Harwood Street to Pacific Avenue/Saint Paul Street; and at the close of the hearing, authorize an ordinance implementing the change (MSS)
- I. Draft Agenda Item: A public hearing to receive comments to amend the City of Dallas Central Business District Streets and Vehicular Circulation Plan to reduce the right-of-way on (1) Harwood Street between Commerce Street and Jackson Street from 85 feet of right-of-way to 64 feet of right-of-way; and (2) Harwood Street between Jackson Street and IH-30 from 78-130 feet of right-of-way to 64-68 feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change (MSS)
- J. Draft Agenda Item: Authorize (1) the rejection of bids for Erosion Control Improvements Group 10 and (2) the re-advertisement for new bids (TWM)
- K. Draft Agenda Item: Authorize (1) the rejection of bids for Erosion Control Improvements Group 11 and (2) the re-advertisement for new bids (TWM)
- L. Draft Agenda Item: Authorize Supplemental Agreement No. 1 to the professional services contract with Half Associates, Inc. for additional surveying, environmental services and preparation of a Municipal Setting Designation and the Final Plat for the Dallas Floodway - Not to exceed \$179,818, from \$640,498 to \$820,316 (TWM)
- M. Draft Agenda Item: A public hearing to receive comments regarding the application for and approval of the fill permit and removal of the floodplain (FP) prefix from approximately 0.788 acres of the current 7.298 acres of land, located at 1717 W. Northwest Hwy, within the floodplain of Elm Fork of the Trinity River, Fill Permit 16-01 (TWM)
- N. Draft Agenda Item: Authorize a construction contract with Jeske Construction Company, lowest responsive bidder of five, for the construction of street and alley paving, storm drainage, water and wastewater main improvements for Project Group 12-3004 (list attached) - Not to exceed \$911,793 (MSS)

Adjourn



Lee M. Kleinman, Chair
Transportation and Trinity River Project Committee

A closed executive session may be held if the discussion of any of the above agenda items concerns one of the following:

1. Contemplated or pending litigation, or matters where legal advice is requested of the City Attorney. Section 551.071 of the Texas Open Meetings Act.
2. The purchase, exchange, lease or value of real property, if the deliberation in an open meeting would have a detrimental effect on the position of the City in negotiations with a third person. Section 551.072 of the Texas Open Meetings Act.
3. A contract for a prospective gift or donation to the City, if the deliberation in an open meeting would have a detrimental effect on the position of the City in negotiations with a third person. Section 551.073 of the Texas Open Meetings Act.
4. Personnel matters involving the appointment, employment, evaluation, reassignment, duties, discipline or dismissal of a public officer or employee or to hear a complaint against an officer or employee. Section 551.074 of the Texas Open Meetings Act.
5. The deployment, or specific occasions for implementation of security personnel or devices. Section 551.076 of the Texas Open Meetings Act.
6. Deliberations regarding economic development negotiations. Section 551.087 of the Texas Open Meetings Act.

Handgun Prohibition Notice for Meeting of Governmental Entities

"Pursuant to Section 30.06, Penal Code (trespass by license holder with a concealed handgun), a person licensed under Subchapter H, Chapter 411, Government Code (handgun licensing law), may not enter this property with a concealed handgun."

"De acuerdo con la sección 30.06 del código penal (ingreso sin autorización de un titular de una licencia con una pistola oculta), una persona con licencia según el subcapítulo h, capítulo 411, código del gobierno (ley sobre licencias para portar pistolas), no puede ingresar a esta propiedad con una pistola oculta."

"Pursuant to Section 30.07, Penal Code (trespass by license holder with an openly carried handgun), a person licensed under Subchapter H, Chapter 411, Government Code (handgun licensing law), may not enter this property with a handgun that is carried openly."

"De acuerdo con la sección 30.07 del código penal (ingreso sin autorización de un titular de una licencia con una pistola a la vista), una persona con licencia según el subcapítulo h, capítulo 411, código del gobierno (ley sobre licencias para portar pistolas), no puede ingresar a esta propiedad con una pistola a la vista."

Transportation & Trinity River Project Committee Meeting Record

The Transportation & Trinity River Project Committee meetings are recorded. Agenda materials are available online at www.dallascityhall.com. Recordings may be reviewed/copied by contacting the Transportation & Trinity River Project Committee Coordinator at 214-671-9502.

Meeting Date: February 13, 2017

Convened: 1:04 p.m.

Adjourned: 2:12 p.m.

Committee Members Present:

Lee M. Kleinman, Chair
Deputy Mayor Pro Tem Erik Wilson, Vice-Chair
Mayor Pro Tem Monica R. Alonzo
Sandy Greyson
Casey Thomas II
Adam Medrano

Presenters:

Timothy McKay, P.E., Executive Vice President, DART
Stephen Salin, AICP, Vice President, DART
Willis Winters, Director, Park and Recreation

City Staff Present:

Jill Jordan
Willis Winters
Sarah Standifer
Rick Galceran
Mark Duebner

Committee Members Absent:

Other Council Members present:

B. Adam McGough

Tanya Brooks, Assistant Director, Mobility and Street Services
Peer Chacko, Chief Planning Officer and Director, Planning & Urban Design

Robert Sims
Louise Elam
Tanya Brooks
Richard Wagner

AGENDA:

Call to Order

1. Approval of the January 23, 2017 Meeting Minutes

Presenter: Erik Wilson, Vice-Chair

Action Taken/Committee Recommendation(s): A motion was made to approve the minutes for the January 23, 2017 Transportation and Trinity River Project Committee Meeting.

Motion made by: Sandy Greyson
Item passed unanimously: X
Item failed unanimously:

Motion seconded by: Monica Alonzo
Item passed on a divided vote:
Item failed on a divided vote:

Transportation and Trinity River Project Committee
February 13, 2017

2. Dallas CBD Second Light Rail Alignment (D2)

Subway Project Refinement

Presenters: Timothy McKay, P.E. Executive Vice President, DART
Stephen Salin, AICP, Vice President, DART

Action Taken/Committee Recommendation(s): Information Only

Motion made by:

Item passed unanimously:

Item failed unanimously:

Motion seconded by:

Item passed on a divided vote:

Item failed on a divided vote:

3. City of Dallas D2 and Streetcar Considerations (Action Item)

Presenters: Tanya Brooks, Assistant Director, Mobility & Street Services

Peer Chacko, Chief Planning Officer and Director, Planning & Urban Design

Action Taken/Committee Recommendation(s): No action was taken by the Committee.

Motion made by:

Item passed unanimously:

Item failed unanimously:

Motion seconded by:

Item passed on a divided vote:

Item failed on a divided vote:

4. 2017 Transportation Alternatives Set-Aside Program Call for Projects (Briefing Memo)

Presenters: Willis Winters, Director, Park & Recreation

Action Taken/Committee Recommendation(s): Information Only

Motion made by:

Item passed unanimously:

Item failed unanimously:

Motion seconded by:

Item passed on a divided vote:

Item failed on a divided vote:

Note: Willis Winters provided a modified copy of the briefing memo to include changing the following:

- 1) "The Davis Advocates, Inc." to "The Circuit Trail Conservancy"
- 2) Changing "2006 Bond Funds" to "2012 Bond Funds"

QUARTERLY REPORTS (Information Only)

5. Trinity River Corridor Bond Program Expenditure Report

6. Major Street Projects Detailed Expenditure Report

7. Department of Aviation Activity Report

8. UPCOMING AGENDA ITEMS

February 22, 2017

- A. Draft Agenda Item: Authorize an Interlocal Agreement with the City of Coppell to allow the City of Dallas to construct, own, and operate a 24-inch wastewater force main pipeline, referred to as the Force Main Project, within the corporate limits of Coppell to serve the Cypress Waters Development in Dallas (DWU)

- B. Draft Agenda Item: Authorize a contract for the construction of grit removal improvements at the Southside Wastewater Treatment Plant - Archer Western Construction, LLC, lowest responsible bidder of three (DWU)
- C. Draft Agenda Item: Authorize Supplemental Agreement No. 1 to the professional services contract with Hayden Consultants, Inc. for additional engineering design of improvements to the intersections of Hampton Road at Canada Drive and Hampton Road at Calypso Street (MSS)
- D. Draft Agenda Item: A benefit assessment hearing to receive comments on street paving, drainage, water and wastewater main I improvements, and alley paving for Project Group 12-3001; and at the close of the hearing, authorize an ordinance levying benefit assessments, and a construction contract with Camino Construction, L.P., lowest responsible bidder of four (MSS)
- E. Draft Agenda Item: A benefit assessment hearing to receive comments on street paving, drainage, water and wastewater main improvements, and alley paving for Project Group 12-3004; and at the close of the hearing, authorize an ordinance levying benefit assessments, and a construction contract with Macval Associates, LLC, lowest responsible bidder of five (MSS)
- F. Draft Agenda Item: Authorize the second step of acquisition for condemnation by eminent domain, if such becomes necessary, from R.K.C.J., L.L.C. to acquire an improved tract of land containing approximately 25,727 square feet, four slope easements containing a total of approximately 23,330 square feet, and a drainage easement containing approximately 5,257 square feet, located on East Wheatland Road near its intersection with Lancaster Road for the Wheatland Road Improvement Project – Not to exceed \$136,944 (\$130,944 plus closing costs and title expenses not to exceed \$6,000) (TWM)

Adjourn (2:12 p.m.)

APPROVED BY:

ATTEST:

Lee M. Kleinman, Chair
Transportation & Trinity River Project Committee

Natalie Wilson, Coordinator
Transportation & Trinity River Project Committee

Memorandum



CITY OF DALLAS

DATE February 24, 2017

TO The Honorable Members of the Transportation and Trinity River Project Committee:
Lee M. Kleinman (Chair), Deputy Mayor Pro Tem Erik Wilson (Vice-Chair), Sandy Greyson,
Mayor Pro Tem Monica R. Alonzo, Adam Medrano, and Casey Thomas II

SUBJECT Urban Transit Design Guidelines

On Monday, February 27, 2017, you will be briefed on Urban Transit Design Guidelines. Copies of the Urban Transit Design Guidelines Document and briefing materials are attached for your review.

Please feel free to contact me if you have any questions or concerns.

A handwritten signature in blue ink, appearing to read 'Alan E. Sims'.

Alan E. Sims
Chief of Neighborhood Plus

Attachments

c: T.C. Broadnax, City Manager
Larry Casto, City Attorney
Craig D. Kinton, City Auditor
Rosa A. Rios, City Secretary
Daniel F. Solis, Administrative Judge
Mark McDaniel, Acting First Assistant City Manager
Kimberly Bizer Tolbert, Chief of Staff to the City Manager
Eric D. Campbell, Assistant City Manager

Jill A. Jordan, P.E., Assistant City Manager
Joey Zapata, Assistant City Manager
M. Elizabeth Reich, Chief Financial Officer
Sana Syed, Public Information Officer
Elsa Cantu, Assistant to the City Manager – Mayor & Council
Directors and Assistant Directors

Urban Transit Design Guidelines

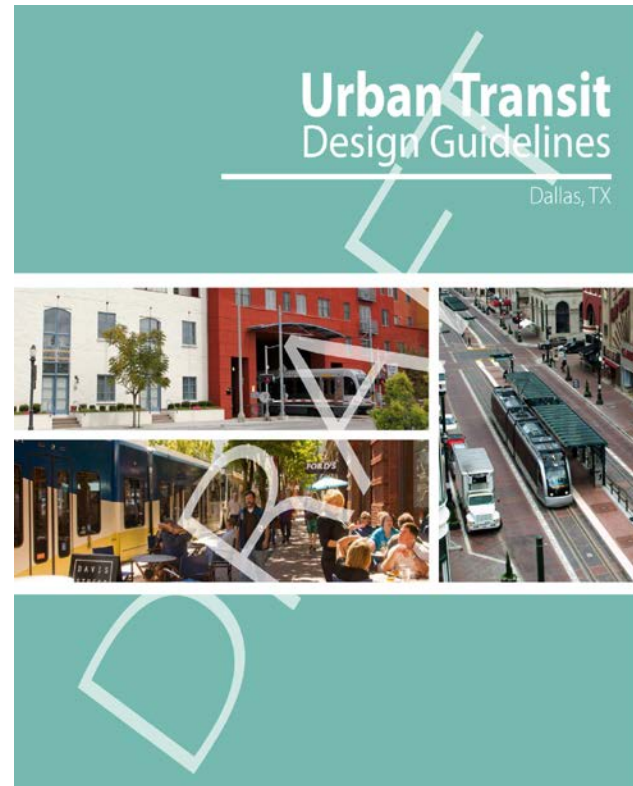
**Transportation and Trinity
River Project Committee
February 27, 2017**

**Peer Chacko, Director
Planning and Urban Design
City of Dallas**



Presentation Overview

- Background
- Purpose
- The Vision
- Applicability and Review Process
- Design Guidelines
- Proposed Action
- Next Steps
- Appendix



Background

- Impetus for *Urban Transit Design Guidelines* came from Downtown stakeholders desire for well designed integration of D2 Light Rail and Central Dallas Streetcar projects into the urban fabric
- Drafted by City staff in partnership with the Downtown Dallas Inc. Mobility Committee with input from DART staff
- Developed based on best practices in consultation with:
 - Downtown Dallas 360 Plan
 - Dallas Complete Street Design Manual
 - Dallas Area Rapid Transit Light Rail Project Design Criteria
 - City of Dallas Tax Increment Financing Districts Urban Design Guidelines
 - National Association of City Transportation Officials (NACTO) *Transit Street Design Guide*

Purpose

- To present draft *Urban Transit Design Guidelines* to be used for review of transit projects in and around Downtown Dallas
- To receive Committee input on the proposed guidelines and review process prior to scheduling Council action

The Vision

- Promote the Downtown Dallas 360 mobility goal of creating a balanced multimodal, regional and local transportation system that:
 - Supports the urban design and livability goals for Greater Downtown Dallas
 - Improves inter-district connectivity for all modes including transit, bicycles, pedestrians and automobiles
 - Does not require a car for short trips, and ensures a safe, well-lit, comfortable and accessible environment for a diverse base of users
 - Encourages mixed use, pedestrian-oriented design and development
 - Responds proactively to future trends in technology, demographics and user preferences

The Vision (continued)

- All future transit projects should emulate the following principles:
 - Creation of pedestrian friendly stations that are accessible, safe, encourage transit ridership and contribute to the public realm through street-level activation and high quality materials and detailing
 - Contribution to a sustainable urban environment by following low impact development standards and incorporating appropriate native landscaping
 - Integration of streetscapes that enhance and encourage pedestrian activity while safely accommodating all modes of transportation
 - Enhancement of economic development potential along transit corridors and transit-oriented development around stations

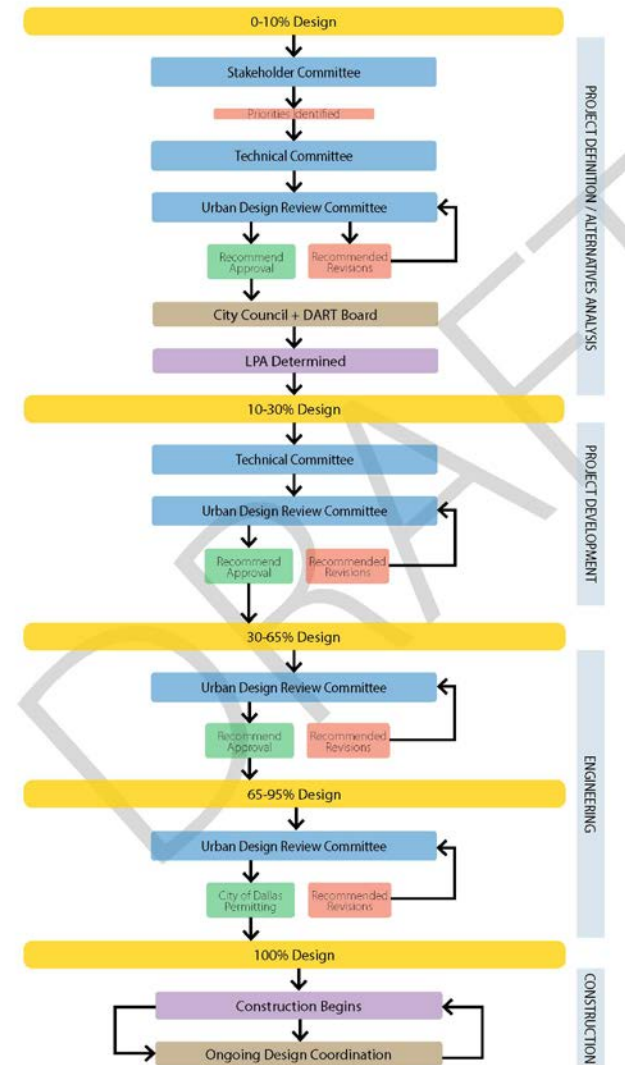
Applicability

- Guidelines are intended to apply to all future transit projects in and around Downtown, and address City of Dallas and DART rights-of-way for the full length of transit project corridors within this geography.
- Guidelines are intended to be advisory (non-regulatory) and to supplement existing DART Design Criteria in order to address quality of the public realm and experience of the transit passenger through a review process.
- City of Dallas and DART would be responsible for ensuring timely review of all project submittals and for active engagement of appropriate stakeholders.
- Application of the guidelines would be the responsibility of the implementing agency (DART) and its design team.

7

Review Process

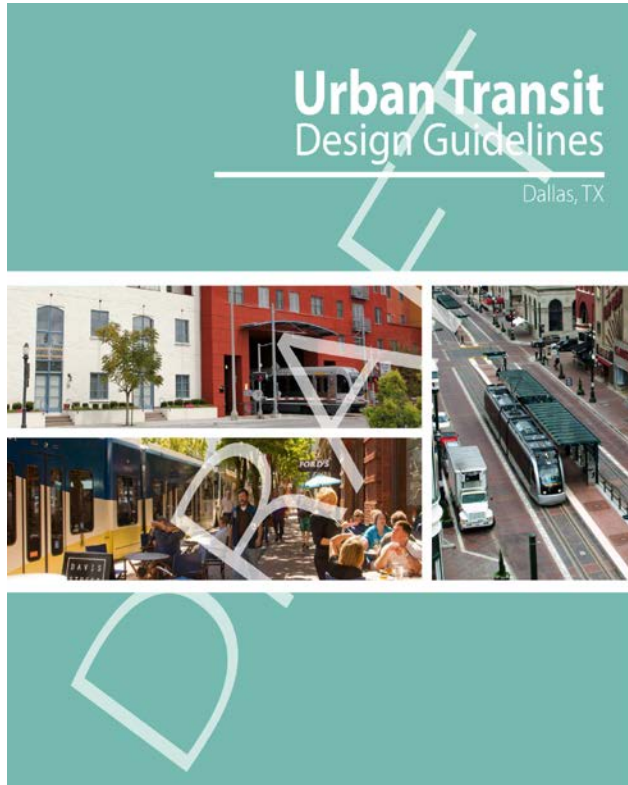
- The review process is integrated into DART’s Alternatives Analysis process by introducing an Urban Design Review Committee to complement DART’s Stakeholder and Technical Committees.
- Prior to determination of the Local Preferred Alignment (LPA), the Urban Design Review Committee role is to provide urban design input on all options.
- After the LPA selection, the Urban Design Review Committee plays an ongoing advisory role at key stages through project development and engineering, in order to facilitate a desirable urban design outcome.



Urban Design Review Committee

- The existing City of Dallas Urban Design Peer Review Committee is recommended to fulfill this role:
 - Has 5 years of experience reviewing private projects that receive City incentives as well as some public street design projects
 - Consists of 9 members representing expertise in the fields of architecture (4), landscape architecture (2), engineering (2), and planning (1)
 - Members are appointed by the City Manager and serve 2-year staggered terms up to a 3-term limit

Design Guidelines



- Station Facility Design
- At-Grade Alignment Design Considerations
- Corridor Design Element Considerations
- Additional Design Considerations
- Adjacent Development Considerations

Station Facility Design

- Underground Stations
 - Station Portal Design
 - Station Portal Placement
 - Platform Design + Amenities
 - Station Artwork
 - Wayfinding
 - Bike Parking and Facilities
- Surface Stations
 - Landscaping
 - Wayfinding
 - Accessibility
 - Lighting
 - Bike Parking and Facilities
 - Station Amenities
 - Layout and Block Design
 - Station/ Shelter Design
 - Passenger vs. Pedestrian Zones



At-Grade Alignment Design Considerations

- At-Grade Alignment Design
 - Street Grid Fabric
 - Development Potential
 - Infrastructure Improvements
 - Stormwater Management



Corridor Design Element Considerations

- Corridor Design Elements
 - Intersection Design
 - Track Design
 - Landscaping
 - Sidewalk Design + Amenities
 - Bike Lanes
 - Paving Materials
 - Train Portals
 - Catenary Poles + Lighting
 - Substations + Service Buildings



Additional Design and Development Considerations

- Additional Design Considerations
 - Land Acquisition and Development
 - Development Air Rights
 - Public Art
 - Sound, Vibration, and Visual Mitigation
 - Operations
- Adjacent Development Considerations
 - Development Character
 - Street and Block Design
 - Pedestrian Level Design
 - Sidewalk and Landscape Design



Proposed Action

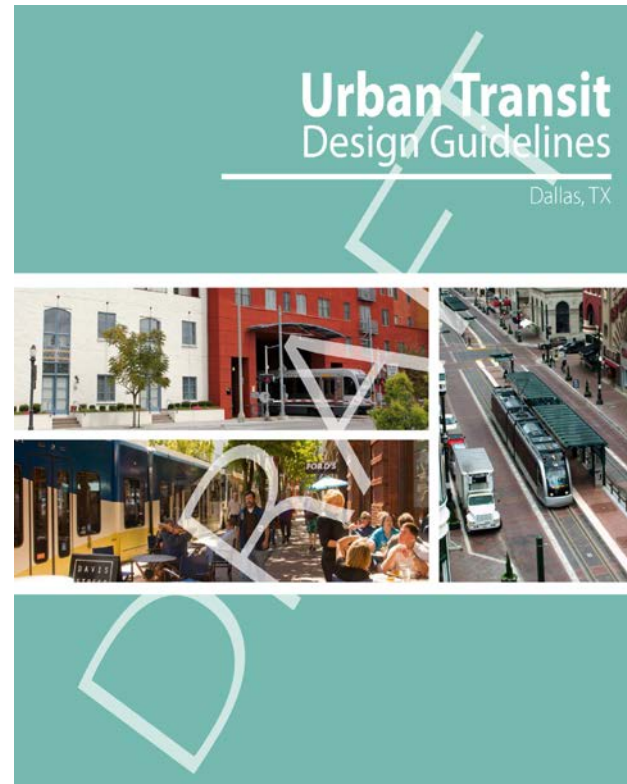
- Adoption of *Urban Transit Design Guidelines* by Council resolution to serve as the basis for design review of transit projects in accordance with Article II Section 6.2 (Applicable City Codes) of the City of Dallas / Dallas Area Rapid Transit Master Interlocal Agreement (ILA)
- Article II Section 6.2 of the ILA provides that DART will comply with all applicable codes, ordinances, permit regulations, review procedures, City plans or other City regulations

Next Steps

- Incorporate Committee input on the guidelines and review process
- Schedule Council adoption of the *Urban Transit Design Guidelines* in April 2017
- Institute the design review process for the D2 Light Rail and Central Dallas Streetcar projects

Appendix

- Draft *Urban Transit Design Guidelines* Document



Urban Transit Design Guidelines

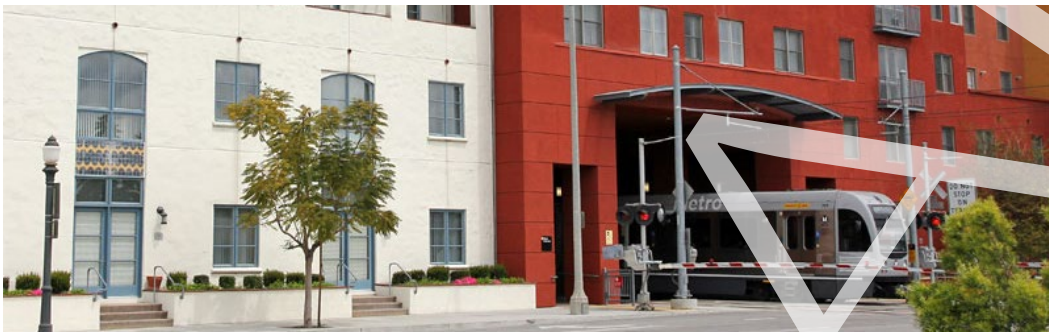
**Transportation and Trinity
River Project Committee
February 27, 2017**

**Peer Chacko, Director
Planning and Urban Design
City of Dallas**



Urban Transit Design Guidelines

Dallas, TX



February 22, 2017





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26 At-Grade Route Alignment Design

30 Corridor Design Element Considerations

38 Additional Design Considerations

44 Adjacent Development Considerations

Acknowledgements

Downtown Dallas Inc

Jacob Browning
Dustin Bullard
John Crawford
Kourtney Garrett
Shalissa Perr
Amy Tharp

Mobility Committee

Brian Adams
Mitch Gatewood
Larry Good
Mark Goode
Jim Harris
Steve Hulse
Patrick Kennedy
Jim Knight
Ramsey March
Craig Melde
Theresa O'Donnell
Tom Persch
Ben Reavis
Scott Rohrman
Steve Salin
Jerry Smiley
Kristian Teleki
Allan Zreet - Committee Chair

City of Dallas

Tanya Brooks
Peer Chacko
Daniel Church
Arturo Del Castillo
Evan Sheets
Jared White

Consulted Documents

City of Dallas, Downtown Dallas Inc. (2011).
Downtown Dallas 360 - A Pathway to the Future

City of Dallas. (2016). *City of Dallas Complete Streets Design Manual*

City of Dallas Office of Economic Development. (2012). *Urban Design Guidelines for Projects located in City of Dallas Tax Increment Financing Districts.*

Dallas Area Rapid Transit. (2005). *Dallas Area Rapid Transit Light Rail Project Design Criteria: Volume 1, Facilities Design.*

National Association of City Transportation Officials NACTO. (2016). *Transit Street Design Guide.*

** Quoted guidelines with citations (ex: NACTO TSDG, 104) refer to the cited page (ex: Page 104) in the NACTO Transit Street Design Guide (2016)*



Vision

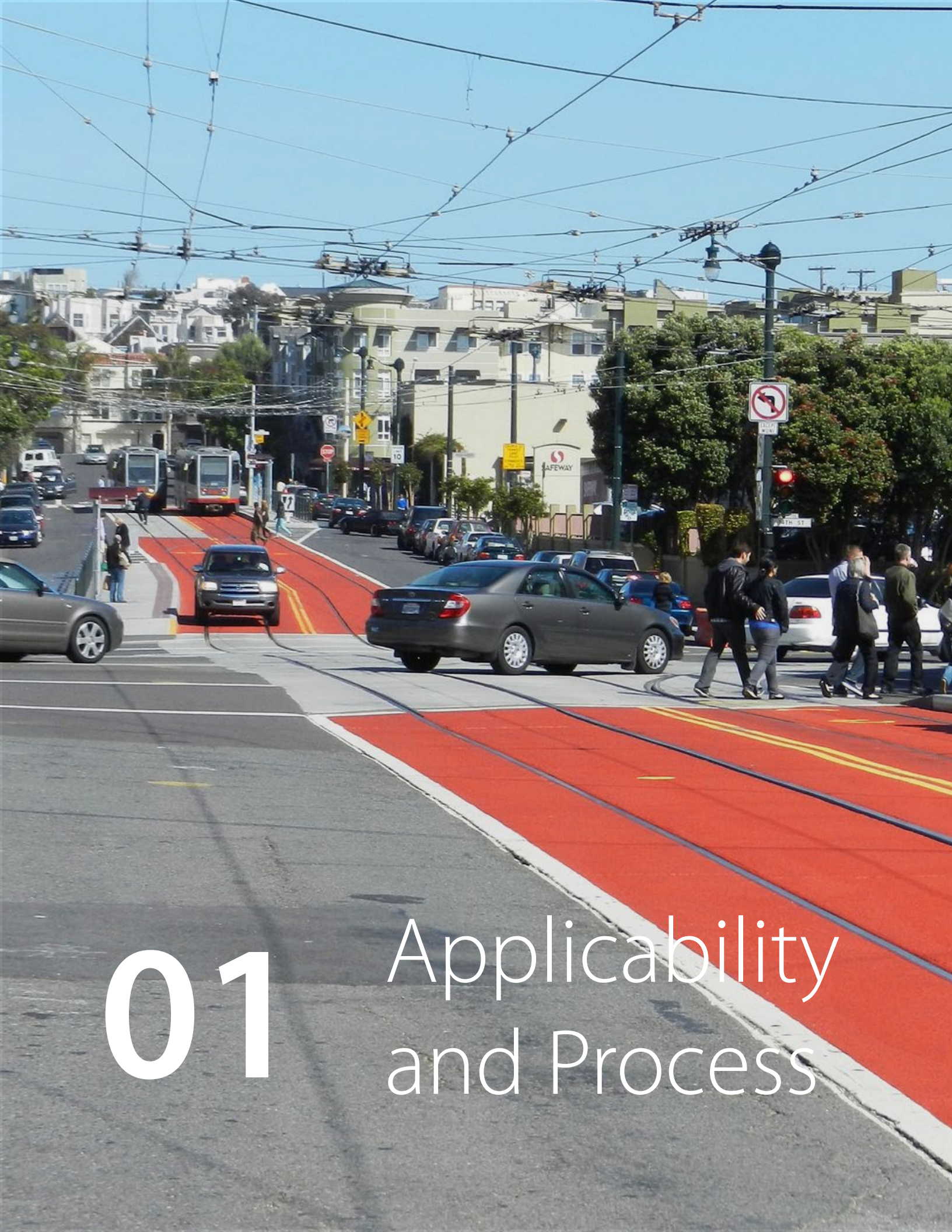
The Downtown Dallas 360 Plan established the mobility goal of creating a balanced multimodal, regional, and local transportation system that supports the urban design and livability goals for the Greater Downtown Dallas area. The intent is to improve inter-district connectivity for all modes, promote alternatives to cars for short trips, encourage mixed-use pedestrian friendly development, and respond to future demographic, user preference, and technological trends.

The success of urban transit corridors in the Downtown Dallas area requires balancing high-quality transit service and an inviting environment through welcoming, high-quality design. Urban transit corridors should establish a unifying and distinct identity that may vary by district. In order to achieve the best and most balanced transit outcomes, all future transit projects should emulate the following principles:

- Creation of pedestrian friendly stations that are accessible, safe, encourage transit ridership and contribute to the public realm through street-level activation and high quality materials and detailing
- Contribution to a sustainable urban environment that follows low impact development standards and incorporates appropriate native landscaping
- Integration of streetscapes that enhance and encourage pedestrian activity while safely accommodating all other modes of transportation.
- Enhancement of economic development potential along transit corridors and transit-oriented development around stations

These principles shaped the *Urban Transit Design Guidelines* and will help produce outcomes that benefit the City through economic development and quality of life, as well as DART through increased density, accessibility, and transit ridership. The *Urban Transit Design Guidelines* are intended to provide policy level design guidance for the development of at-grade and below-grade transit corridors and stations in and around Downtown Dallas. Though non-prescriptive in nature, the guidelines establish expectations for the quality of the urban environment in the vicinity of the transit corridors, based on best practices.

The goal is to provide guidance and direction to the urban transit project design team at key decision points in order to ensure desired urban design outcomes and to help facilitate consensus among project stakeholders.



01 Applicability and Process



- 8 Applicability
- 8 Process
- 9 Process Diagram



Applicability

The Urban Transit Design Guidelines outlined in this document apply to all future below and at-grade urban transit corridors in and around Downtown Dallas. They are intended to address the City of Dallas and DART rights-of-way for the full length of the transit corridor, and also address design considerations from adjacent building-face to building face where applicable. Applying these urban design guidelines to future urban transit projects is imperative to creating a city that is inviting, pedestrian-friendly, and transit-friendly.

The Guidelines are intended to be advisory (non-regulatory) and serve as a companion document to the DART Design Criteria. The DART criteria address system compatibility, function, operations, safety and maintenance, while the Guidelines address the quality of the public realm and experience of the passenger. The Downtown Dallas 360 Plan, Dallas Complete Streets Design Manual, and TIF District Urban Design Guidelines are also intended to serve as companion reference documents.

These guidelines are an important piece of the development process, but do not replace the need for continued coordination and partnership between City of Dallas, development partners, and other stakeholders and agencies. It is also not the intent of this document to supercede or take the place of input by affected neighborhoods and/or stakeholders along transit project corridors. While this document provides important guidance and sets expectations for development of urban transit projects, each site and project will have its unique differences. Successful development can only occur if there is ongoing communication between all parties, and a commitment to good design.

Process

The following process establishes the role of an Urban Design Review Committee and describes the review procedures and expectations for application of the *Urban Transit Design Guidelines*.

The review process has been integrated into DART's typical Alternatives Analysis process by introducing an Urban Design Review Committee to complement DART's Stakeholder and Technical Committees. Application of the *Guidelines* shall be the responsibility of the implementing agency and respective project design team. The City of Dallas and DART will ensure timely urban design review at key decision points during the alternatives analysis, project development, and engineering phases of transit projects. The *Guidelines* should also be used in the development of all design, master planning, site planning and construction documentation activities.

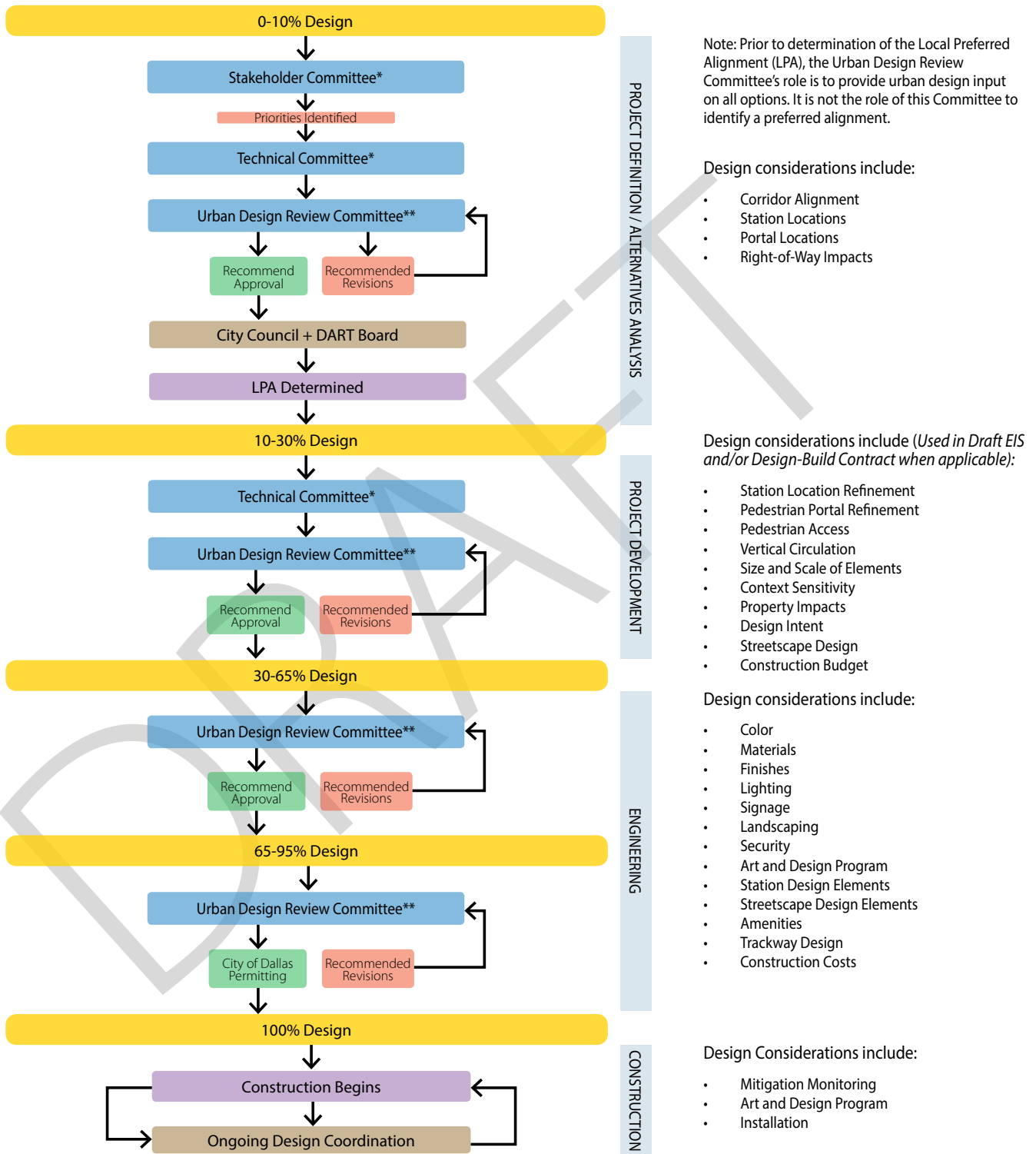
The role of the Urban Design Review Committee (see diagram) will be fulfilled by the City of Dallas' existing, City Manager-appointed, Urban Design Peer Review Panel, which currently performs review functions on private projects receiving City of Dallas incentives as well as public street projects. Prior to determination of the Local Preferred Alignment (LPA), the Urban Design Review Committee's role is to provide urban design input on all options. It is not the role of this Committee to identify a preferred alignment. After the LPA selection, the Urban Design Review Committee plays an ongoing advisory role at key stages through project development and engineering, in order to facilitate a desirable urban design outcome.

After construction begins, City of Dallas and DART staff will have a continuing role to ensure design coordination, particularly for design-build contracts.

Applicability and Process



This process lays out general expectations for the design review process, using the D2 planning and engineering process, as an example. This process may be modified on a project-by-project basis as needed.



* "Stakeholder Committee" and "Technical Committee" represent the committees organized by DART as a component of the public input process for choosing the D2 locally preferred alignment alternative

** Urban Design Review Committee role will be fulfilled by the City of Dallas' Urban Design Peer Review Panel

02

Right-of-Way Design Guidelines

TRI MET MAX

Transit Tracker
7608

Sept 18:
MAX Green Line Service Begins

Aug 30:
MAX Yellow Line Service Begins

GREEN MEANS GO

Sept 12:
MAX Green Line Grand Opening

1.1 MAX System

Be alert and stay safe
If you see suspicious behavior or a suspicious package, call a transit watchdog at 503.233.3333

SW 5th &
Jefferson

● ●

Southbound

City Hall
SW 5th &
Jefferson

● ●

Southbound

Bus

D

SW Madison ST

12	Station Facility Design
14	Underground Station Facility Design
20	Surface Station Facility Design
26	At-Grade Alignment Design
30	Corridor Design
38	Additional Design Considerations
44	Adjacent Development Considerations





Station Facility Design

Underground Station Facility Design

- 15 Station Portal Design
- 15 Station Portal Placement
- 16 Platform Design + Amenities
- 17 Station Artwork
- 17 Wayfinding
- 18 Bike Parking and Facilities

Surface Station Facility Design

- 21 Landscaping
- 21 Wayfinding
- 22 Accessibility
- 22 Lighting
- 23 Bike Parking and Facilities
- 23 Station Amenities
- 24 Layout and Block Design
- 25 Station/ Shelter Design
- 25 Passenger vs. Pedestrian Zones



Underground station design should integrate seamlessly with the surrounding public realm with pedestrian portal connections designed to provide high quality public places. The relationship of the station to any surrounding development must also be considered to ensure a positive integration that opens up views, sightlines and maximizes connectivity to adjacent development. Subway stations, specifically their portals, should be designed to either fit elegantly and seamlessly into the surrounding neighborhood's character or to make bold architectural statements of their own.

Unlike any other building typology, subway stations are buildings where the distinction between indoor and outdoor is ambiguous. These transitory spaces are complex and require functional efficiency as a minimum to bring order, balance, elegance, and coherence to satisfy the public. Due to this busy and sometimes chaotic station environment, the following environmental factors should be given due consideration:

Light - carefully consider the artificial lighting while also maximizing and controlling the amount of natural daylight in the station.

Color - the palette of colors must be calm and also compatible with the branding of the station. For people with visual impairments, surfaces must not be too reflective and vertical planes must be well contrasted from horizontal surfaces.

Sound - acoustics of the spaces must be well considered and adapted to the use of public announcement systems.

Spatial Volume – The spatial volumes must be well proportioned, appropriate for their use and feasible to maintain. Underground stations, pedestrian portals, and transition spaces have an impact beyond their immediate boundary and should be regarded as place makers which influence the local economy, its cultural identity and environmental wellbeing.



Station Portal Design

- Design underground station entrance portals to compliment the surrounding architectural character of the area, paying particular attention to historic districts.
- Scale the size of the portal to the projected amount of boarding/arrivals at station.
- Provide easily accessible elevator entries that compliment surrounding architectural context.
- Consider providing secure entry portals with paid patron control.
- Provide bicycle stairways at all stations to ease cyclists and relieve congestion at elevators.
- Elevators should be co-located with primary entrance portal (stairs/ escalator) to the extent possible.

Station Portal Placement

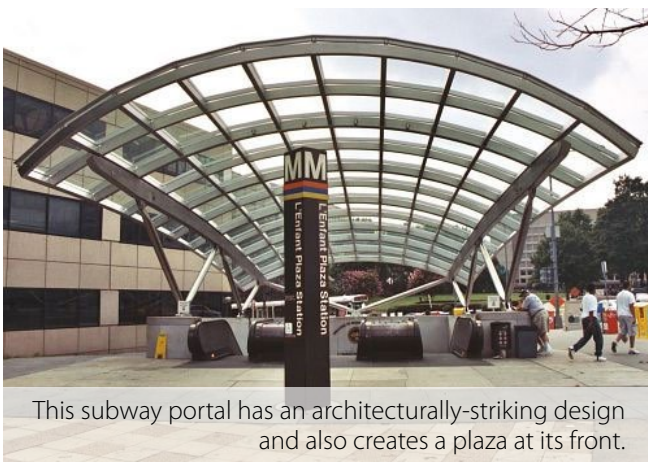
- Station portals that are incorporated into the facade or design of new or existing buildings or adjacent to existing parks or plazas are preferred to portals within existing street right-of-way. Connect station access to building lobbies when feasible
- Ensure that station portal does not obstruct pedestrian clear zone of at least 7'.
- Place fare stations either inside station or in space that provides at least 4' of queuing outside of the pedestrian clear zone in accordance with DART Design Criteria.
- Locate station portals in a manner that supports wayfinding and encourages easy transfer to other transit modes, including bus and streetcar.
- Locate portals in proximity to other transit modes and provide clear wayfinding at-grade to entrance portal locations.



This subway portal maintains a wide sidewalk adjacent to portal while also utilizing a high-quality architectural design.



This subway entry in Seattle is incorporated into the facade of a building above the station.



This subway portal has an architecturally-striking design and also creates a plaza at its front.



This subway portal in New York City has distinct features that enhance the plaza and the adjacent building



Platform Design + Amenities

- Provide a spatial volume appropriate to the station function that creates a sense of openness and place.
- Encourage design of underground platforms to reflect the station location and nearby institutions above ground.
- Encourage distinct and durable materials for the platform in accordance with DART Design Criteria.
- Provide adequate seating.
- Allow for cellular service providers to provide high-quality cell service and wi-fi in tunnels and at stations.
- Utilize color and light to create interest.
- Provide high-levels of light to ensure rider safety and comfort.



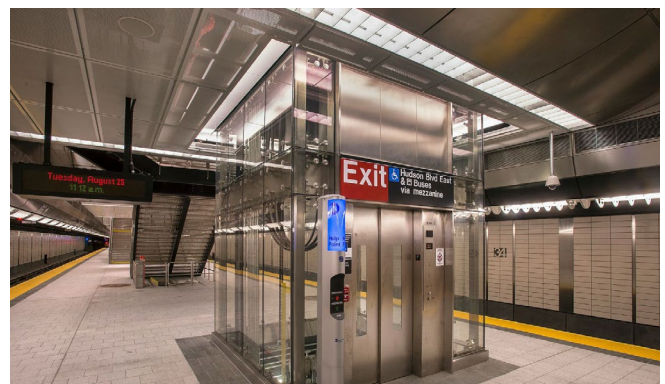
A subway station in Toronto, Ontario incorporates characteristics of the above-ground museum.



A use of high-quality materials in a subway station in Munich, Germany



Art and bright lighting is integrated into station design in Auckland, New Zealand



Wide platforms and a centrally-located elevator make this platform in New York City safe for all users



Station Artwork

- Encourage and program the placement of public artwork in station portals as well as at station platforms, avoiding conflicts with passenger operations
- Incorporate public artwork into the design of the station platform in accordance with DART Design Criteria.
- Integrate public art with architectural elements and materials to leverage budget.
- Involve City of Dallas Office of Cultural Affairs with the DART Art Program and the selection of artists.



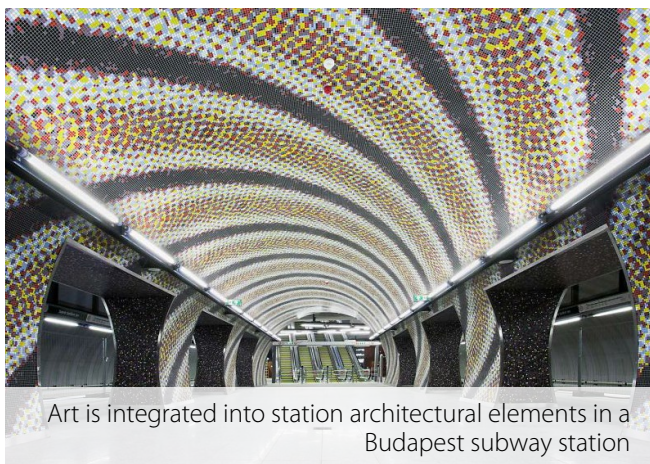
Public art is integrated into station design at Seattle's Capitol Hill light-rail station

Wayfinding

- Place wayfinding in predictable locations, such as overhead or at eye-level, at regular intervals and near intersections. NACTO TSDG, 101
- Include relevant transportation connections and services, including regional routes and bike share stations, to expand rider options. NACTO TSDG, 101
- To direct riders to and from stations to destinations in the station area, indicate travel direction and times in easily understood units such as walking time. NACTO TSDG, 101
- Provide area maps at all stations.
- Wayfinding should include clear, simple guidance to nearest street intersections, landmarks, and points of interest.



Simple wayfinding with maps are important for helping travelers unfamiliar with a station and the area



Art is integrated into station architectural elements in a Budapest subway station



This wayfinding in Tokyo is beautiful and simple, making transfers easy for all users



Bike Parking + Facilities

- Consider providing a manned Bike Station at one key light-rail station in each district. The bike station should be immediately accessible from the station platform or portal.
- Bike parking and facilities are applicable to both underground and surface light-rail station design.
- Provide a clear zone around bicycle parking to avoid impeding traffic on adjacent sidewalks.

NACTO TSDG, 105

- Short-term bike parking should ideally be located within 50' of stop or station entrance.
NACTO TSDG, 105
- Bike storage “shells” should not be used in urban areas or along sidewalks.



The Union Station METRO in Washington DC has a large, protected bike storage facility



The exterior of the Washington DC Union Station METRO bike storage facility



New York City has numerous smaller, covered bike parking facilities adjacent to their subway station entries.



Small parcel cuts caused by track alignment near stations are great locations to place overflow bike parking





Surface station design should integrate seamlessly with the surrounding public realm through sidewalks and plazas designed to provide high quality public places. The relationship of the station and any surrounding development must also be considered to ensure a positive integration that encourages connectivity and transit use.

Surface stations should be seamless with the surrounding urban environment while also providing a well-defined and secure “transit space” that identifies the boarding area and related amenities. The distinction between public space and the transit function should be defined in a manner that provides consideration of the following:

Clear Lines of Sight - Crime Prevention Through Environmental Design (CPTED) principles should be followed to provide a sense of security and safety.

Definition of Space – the functional platform boarding area should clearly defined to the passenger by shelters, planters, railings, amenities, pavement treatments and subtle changes in materials.

Environmental Protection – canopies, trees and other elements should provide station identity and enhance passenger comfort through protection from sun, wind and rain.

Function and Design – Stations should convey an image of simplicity in function and modern timeless design.



Landscaping

- A high importance should be placed on increasing the “urban forest” through the introduction of trees at regular intervals, wherever possible.
- Provide landscaping and irrigation at platforms with street trees at a minimum of 30’ on center where possible. Additionally, consider tree trenching and structural soil to allow for maximum canopy.
- Consider other methods of shade provision including but not limited to canopies, awnings, and other aesthetically appealing structures.

Wayfinding

- Place wayfinding in predictable locations, such as overhead or at eye-level, at regular intervals and near intersections. NACTO TSDG, 101
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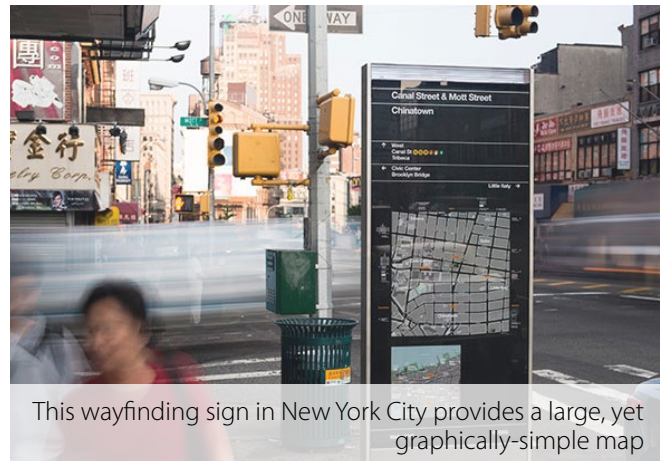
This transit mall in Portland has frequently planted trees, providing shade and beauty for waiting passengers



These wayfinding signs provide both an easy-to-read map and up-to-date bus arrival times



This transit mall in San Jose has frequently spaced street trees that produce a pedestrian-friendly street



This wayfinding sign in New York City provides a large, yet graphically-simple map



Accessibility

- For riders with visual disabilities, provide an alternative to visual display boards. Audible announcements are preferred over braille and other methods that require finding the display. Consider station/street noise and environmental characteristics during implementation. NACTO TSDG, 101
- Where pedestrian crossings traverse tracks and bus transitways, use audible warnings to ensure all pedestrians are aware of oncoming transit vehicles. NACTO TSDG, 32
- Stations and adjacent improvements must meet all Texas Accessibility Standards.
- Where passengers using wheelchairs are directed to specified doors, ensure the accessible doors are clearly communicated throughout the boarding platform using signs and markings. NACTO TSDG, 67



Many bus stops in Washington DC have push buttons reporting bus arrival times for the visually impaired

Lighting

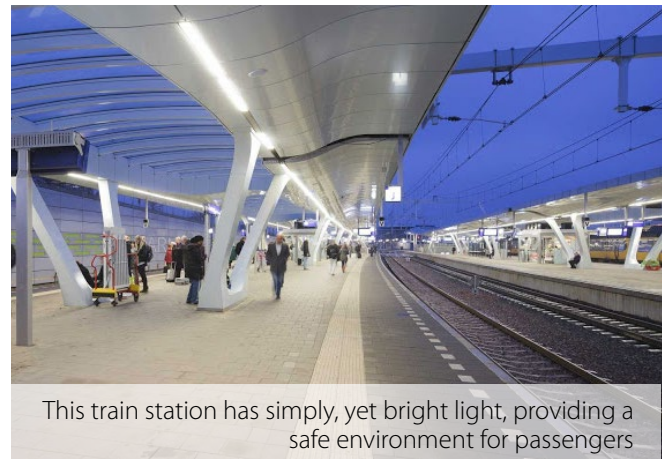
- Use pedestrian-scale lighting, typically including lamps less than 25 feet high, to increase comfort and safety around stops. NACTO TSDG, 67
- Lighting shall be “cut-off” type to avoid illuminating the sky and surrounding development.
- Higher illumination around transit stops should be gradual rather than sudden to avoid creation of virtual shadows as driver and bicyclist eyes adjust. NACTO TSDG, 67



This light-rail station in Charlotte provides bright, white light incorporated into the train platform roof



Ensuring easy loading for all riders is imperative. At-grade loading such as this is preferred



This train station has simple, yet bright light, providing a safe environment for passengers



Bike Parking + Facilities

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NACTO TSDG, 105
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NACTO TSDG, 105
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The Union Station METRO in Washington DC has a large, protected bike storage facility

Station Amenities

- Avoid using materials for benches and other sitting areas that retain heat.
NACTO TSDG, 98
Additionally, provide shade for all sitting areas.
- Seating shall not conflict with paths, leaving 6' of clear distance on all sides where pedestrians are expected.
NACTO TSDG, 98
- Install appropriate amount of seating for expected demand at each station.
- Provide passengers with amenities at station area, including seating, trash cans, wayfinding, etc. compatible with the corridor theme.
- Trash cans, seating, and other objects must not block accessible path and boarding areas.
NACTO TSDG, 99
- Integrate status displays and visual message boards into corridor design elements.



This station platform contains numerous seating options, an open and transparent design, and an arrival board



New York City has numerous smaller, covered bike parking facilities adjacent to their subway station entries.

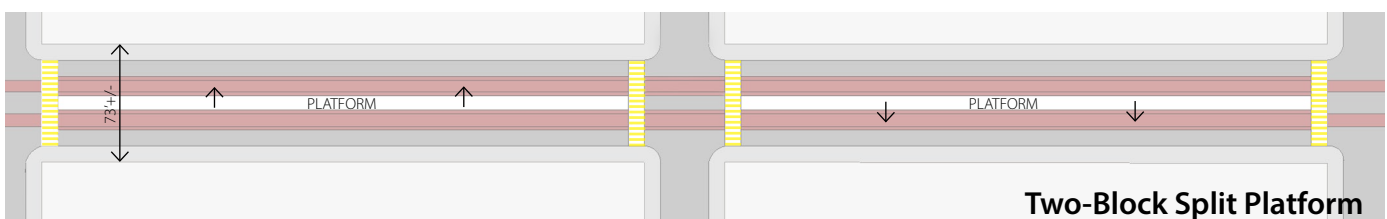
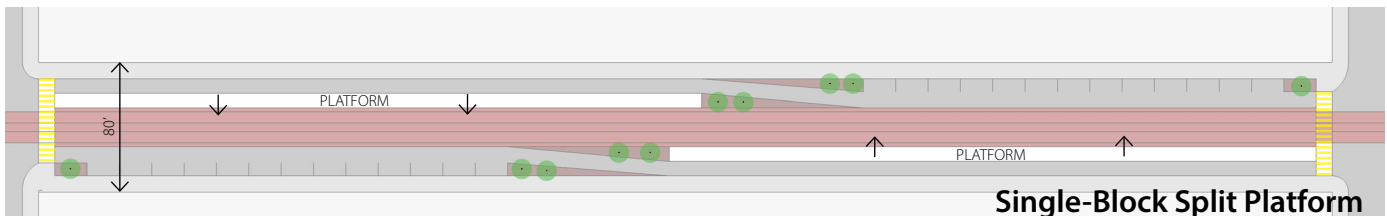


This station platform has a large and shaded canopy, seating, wayfinding, and historical education elements



Layout and Block Design

- Consider staggering at-grade light-rail platforms between blocks on narrow right-of-way streets, minimizing overall right-of-way required for station.
- Boarding bulb stops for streetcar should be considered where vehicle operates in offset lanes without rightside bike facilities. Boarding bulbs can be installed at near-side, far-side, and mid-block stops, at both signalized and unsignalized locations. NACTO TSDG, 70
- At all stops, provide at least 10' of clear sidewalk space, ahead of transit vehicle at near-side stops and behind transit vehicle at far-side stops. NACTO TSDG, 71
- If shelters are placed on boarding bulbs, they must be placed clear of front and back-door boarding areas. NACTO TSDG, 71
- An accessible boarding area, typically 8' x 5' long, must be provided to permit boarding maneuvers by a wheelchair, generally requiring islands to be at minimum 8' wide. Islands with railings along the rear side will require an extra foot of space, making total width 9'. NACTO TSDG, 83
- Ensure that pedestrian refuge islands crossing transitways are wide enough to allow crowds of people to wait, particularly near stations. Place detectable warning strips on both sides of every flush pedestrian crossing. NACTO TSDG, 83
- Railings shall be installed along platforms adjacent to the through lane to control pedestrian access and discourage dangerous crossings. Channelize pedestrian movements to platform entrances with enhanced crossing treatments. NACTO TSDG, 83





Station/ Shelter Design

- Shelter design should reflect the architectural characteristics of the surrounding neighborhood while also maintaining an identity established by DART for the corridor.
- Platform shelters must be permeable to allow for freedom of movement between each side of platform or adjacent sidewalk.
- Platform windscreens are not encouraged in order to reduce visual clutter, improve movement when boarding/ off-boarding, and limit vandalization potential.
- Placement of shelters' supporting posts/walls must not conflict with accessible travel paths, boarding areas, or transit vehicle door zones. NACTO TSDG, 16



This light-rail platform in Salt Lake City is permeable, allowing for free pedestrian movement at the station



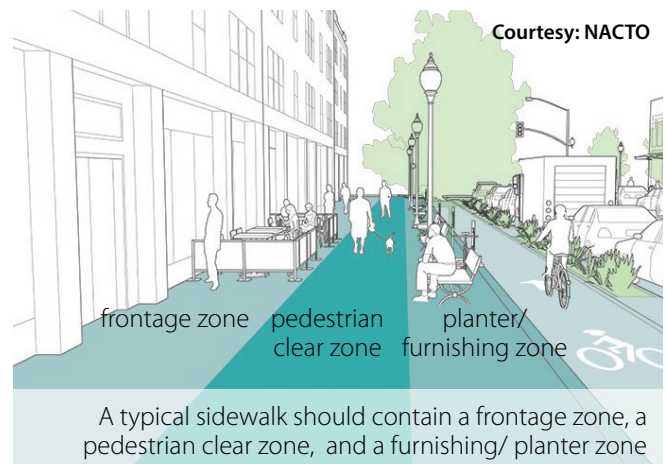
This light-rail station in Austin is both permeable and has a unique and distinct paver pattern

Passenger vs. Pedestrian Zones

- Shelter placement must allow a minimum of 6' through-path between obstructions and warning strips at platforms and around all sides when at the level of the sidewalk, or around the street side if on an elevated platform. NACTO TSDG, 16
- Sidewalk adjacent shelters/ platforms should allow for minimum of 7' pedestrian clear zone in the rear in adherence with Complete Streets Manual and Downtown Pedestrian Overlay. Dimensions may vary by location context.
- Clearly delineate passenger zones from pedestrian zones and provide fully accessible pedestrian areas with sloped walkways in lieu of stairs or ramps



This transit mall has numerous amenities such as trees and trash cans, yet keeps them out of the pedestrian zone



A typical sidewalk should contain a frontage zone, a pedestrian clear zone, and a furnishing/ planter zone





At-Grade Route Alignment Design

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- 28 Development Potential
- 29 Infrastructure Improvements
- 29 Stormwater Management

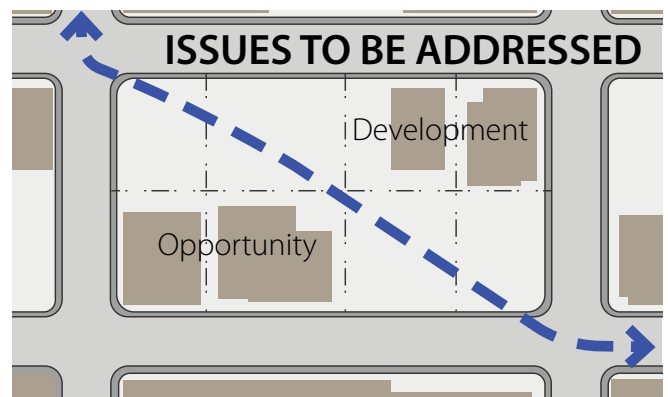
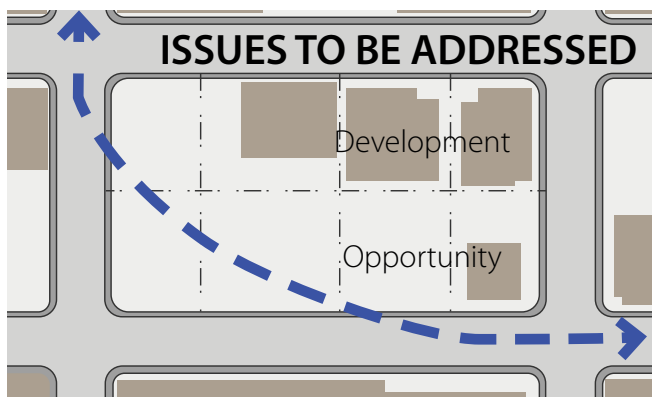


Street Grid Fabric

- Maintain the existing street grid to the extent possible by minimizing closure or interruption of existing streets that cross the rail alignment.
- Maximize extent to which DART light rail operation can occur within right-of-way when at-grade.

Development Potential

- Ensure maximum development potential of or enhancement to adjacent full and partial parcels by minimizing impact to development parcels used for rail operations.
- Provide mitigation of undesirable property conditions and partial parcels created by the alignment.
- Air rights development strategies should be pursued when ideal track alignment is not possible in order to maximize development opportunity.



At-grade Rail alignment should consider the possibility for adjacent development to occur. This diagram demonstrates ideal alignment conditions as well as rail conditions that would need improvement or mitigation.



Infrastructure Improvements

- Integrate infrastructure improvements with existing development adjacent to corridor through materials used and/or through physical design.
- Coordinate with other public capital projects and private development infrastructure improvements to extent possible.
- Assign a City of Dallas utilities facilitator to act as a point person to ensure consistent coordination between DART and the various city departments.

Stormwater Management

- Provide integrated stormwater management (iSWM) adjacent to and incorporated with tracks where possible.
- Provide bioswales with a slight longitudinal slope that moves water along the surface to allow sediments and pollutants to settle out. In place infiltration then allows localized groundwater to recharge. NACTO TSDG, 104
- Bioswales should drain within 24 hours; this is especially critical near transit stops where pooling can degrade transit access. NACTO TSDG, 104
- Incorporate updated City of Dallas Pavement and Drainage Manual requirements into stormwater management design for transit corridors.



This Portland infill development has integrated infrastructure improvements with the adjacent rail



Light rail in Portland is built with iSWM adjacent to many of their tracks



The light rail in Phoenix helped foster adjacent development such as the ASU Journalism School.



Light rail in Salt Lake City is also built with iSWM adjacent to many of their tracks





Corridor Design Element Considerations

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Intersection Design

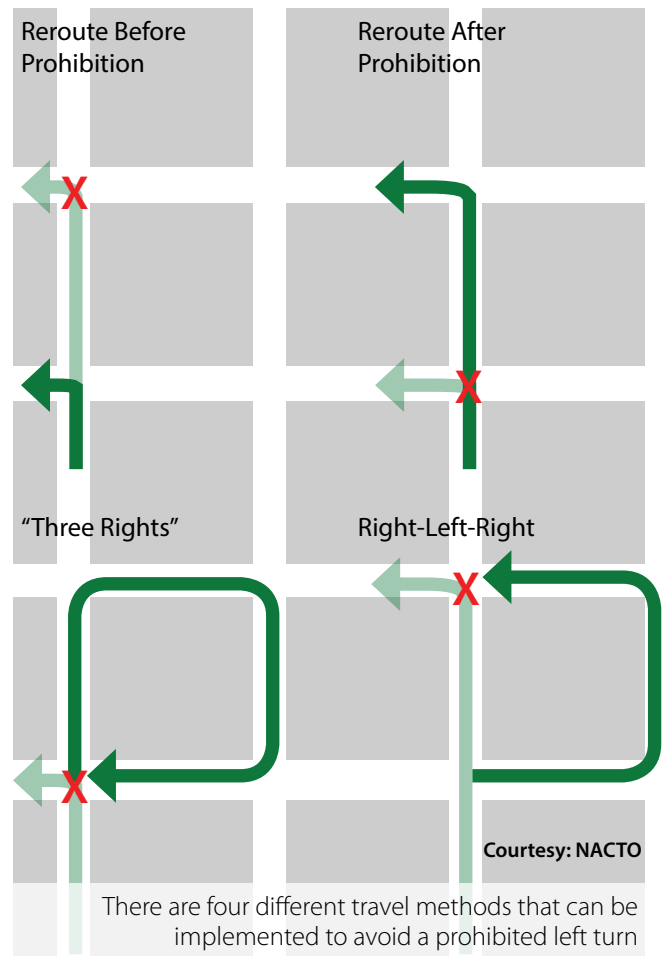
- For center-running fixed guideway transit, at intersections, use separate turn phases, prohibit left turns across median transitway, or prohibit left turns when transit vehicles are present. Extend vertical elements to the intersection edge where turns are prohibited. Use transit signals and either active Transit-Signal Prioritization or transit signal progressions (TSP). NACTO TSDG, 32
- Crosswalks must be accessible, with special attention to both people using wheelchairs and wheeled mobility devices and people with no or low vision. Curb ramps must be provided at all street crossings that involve a change in grade. Do not obstruct the top of the curb ramp. NACTO TSDG, 66
- All intersections with pedestrian, bicycle, or motor vehicle traffic must be signalized. To avoid conflicts with transit vehicles, left- and right-turning traffic across the transitway must be either prohibited or accommodated using turn lanes with dedicated signal phases. Additionally, consider TSP for intersections along route. NACTO TSDG, 130
- Clearly designate mid-block crossings where applicable to ensure pedestrian safety.
- Bike lanes shall cross tracks as close to 90° as possible (at a minimum 60°) to ensure safety for cyclists. NACTO TSDG, 166



This bike lane crosses the light rail track at a near 90° angle, protecting cyclists from falls



This light-rail platform leads to a large crosswalk that is paved with a different, distinguishable material





Track Design

- Consider a shared transitway for non light-rail modes in places where a low-curb or rumble strips increase pedestrian permeability across the entire street, maximizing available public space and emphasizing the shared condition of the street. Pedestrians can cross a shared transit street at any point, but are discouraged from walking along the central transitway by the high volume of transit vehicles. NACTO TSDG, 28
- Auto traffic is either prohibited or limited using volume management techniques that filter out thru-traffic and permit local vehicle access, especially for deliveries. NACTO TSDG, 29
- A minimum width of 12' and a maximum width of 15' for each LRT lane (24'-30' for dual-running lines) and maximum width of 11' for each streetcar lane should be adhered to.
- To avoid conflicts with center-running transit vehicles, left turns should be prohibited, or accommodated using left-turn lanes and dedicated signal phases. NACTO TSDG, 119
- Use either rumble strips or low vertical elements between travel lanes and DART lanes. NACTO TSDG, 137



This light rail runs slowly through a plaza in Germany, creating a functioning plaza that is safe for pedestrians



The light rail in Houston has large truncated domes protecting the transit lane from the travel lane



The light rail in downtown Denver runs along side vehicles, functioning much like a streetcar



This light rail has rumble strips adjacent to the tracks to warn motorists not to enter the lane



Landscaping

- Consider providing a continuous green space between tracks and adjacent road beds if appropriate. Discontinue green space at intersections and pedestrian crossings, and provide accessible paths for pedestrians through the transitway. NACTO TSDG, 133
- Consider providing landscaping & irrigation along track where possible, with street trees at a minimum of 25' on center where possible. Additionally, consider tree trenching to improve canopy.



Landscaping beneath the tracks and along the tracks is preferred due to aesthetics and stormwater management



These light rail tracks run above grass, increasing pervious surface while also improving overall aesthetics

Sidewalk Design + Amenities

- Provide street furniture (bollards, benches, planters, street lights, bicycle parking, etc) to define shared space and integrate it into the cohesive street design. Design elements provide guidance for the visually impaired and delineate the traveled way from the pedestrian-exclusive area. Where less permeability is desired, such as just ahead of stations, use plantings, railings, and furniture to concentrate activity in desired areas and channelize pedestrian travel paths. NACTO TSDG, 28
- Sidewalk design/width and clear width to be compatible with City of Dallas sidewalk requirements for the CBD, the City of Dallas Complete Streets Design Manual, and the Downtown 360 Street Typology Guidelines.



A wide sidewalk, ideally with street trees and lighting, is preferred to protect pedestrians from the adjacent train



San Jose's transit mall has numerous sidewalk amenities including trees, planters, trash cans, and newspaper racks



Bike Lanes

- Integrate bike lanes with transit corridors and clearly designate bikeway from automobiles and transit where applicable. Bicycles may also be accommodated in shared travel lanes where speeds are low. Accommodate bicycle turns at right angles using two-stage turn queue boxes. Where motor vehicle through-traffic is prohibited, consider providing a cycle track through the plaza. NACTO TSDG, 33
- Consider bi-directional bike lanes paralleling the DART alignment on one-way streets.



These bike lanes slip behind a train and bus platform, protecting cyclists and providing a buffer to the sidewalk

Paving Materials

- The trackway should be designated using red or alternate, distinct color to deter drivers from entering the guideway. Also consider using distinct pavers. NACTO TSDG, 134
- Durable materials such as brick, stone, and unit pavers shall be used on the transitway or across the entire right-of-way. NACTO TSDG, 32 No non-traversable surface such as ballast shall be used.
- Provide special paving at street intersections. Consider the durability of materials based on lessons learned on the Downtown Transit Mall.
- Provide quality unit paver or integral colored concrete at pedestrian areas.
- Paving materials are to be used to clearly define transit /pedestrian/vehicle areas.



Red-painted transit lanes let drivers know that the lane is not for driving



These bike lanes are well-marked and physically separated, protecting cyclists from the rail and cars



The light rail in downtown Houston incorporates well-designed brick paving that provides a cohesive feel



Train Portals

- Consider locating stations adjacent to entrance to train portal. Additionally, consider placing portal in center of road/ boulevard.
- Shield entrance to tunnel with decorative, yet protective fencing and roof cover as well as buffering with landscaping.
- Consider making excess land adjacent to tunnel portal a harmoniously designed open space, plaza, or activated, developed parcel.
- Encourage air rights development over portals.
- Provide aesthetically appealing safety measures to prevent access into tunnel by pedestrians and vehicles.



This tunnel portal in San Francisco is incorporated into an adjacent park

Egress + Ventilation Design

- Fully integrate ventilation and emergency egress structures into new or existing development where possible.
- Full screen any ventilation structures in the right-of-way or provide flush gratings. Do not place gratings in sidewalks where possible.
- Where ventilation shafts cannot be incorporated into new or existing construction, provide context sensitive architectural design and incorporate artwork where possible.



Disguised emergency egress such as this trap door in the sidewalk are preferred



The light rail along the Embarcadero in San Francisco descends underground in the middle of the street



This emergency egress and service building is disguised through artful design in an existing park



Catenary Poles + Lighting

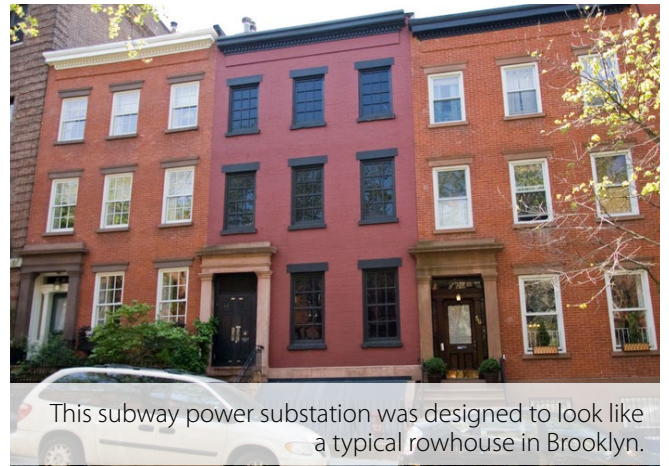
- Catenary poles and lighting are to be integrated wherever possible to avoid cluttering right-of-way.
- Catenary poles should fit the surrounding neighborhood context and should introduce minimal visual intrusion.



Catenary poles and wiring should incorporate street lighting and be aesthetically appealing

Substations + Service Buildings

- Do not place service buildings adjacent to development unless fully contained within development.
- Fully screen all service buildings with masonry walls or landscaping.
- Place service buildings on parcels that do not preclude future development.



This subway power substation was designed to look like a typical rowhouse in Brooklyn.



These catenary poles are artful and incorporate lighting into their design



The proposed subway service building matches the architectural character of the surrounding district



R & J
**JEWELRY
DIAMONDS**
BUY-SELL-PAWN
**MONEY TO LOAN
ON ALL JEWELRY**
JEWELRY REPAIR
WATCH REPAIR & BATTERIES
APPRAISALS

VTA
3:30 PM



Additional Design Considerations

Land Acquisition and Development

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Sound, Vibration, and Visual Mitigation

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Operations

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- 43 Durability + Maintenance



Pocket Parks + Plazas

- Utilize remaining parcels and partial takes along corridor as opportunities to develop pocket parks or plazas as appropriate to land use.
- All pocket parks and plaza shall provide shade and other comfortable amenities for users.
- At stops/ stations/ station portals, provide public plazas and/or incorporate station portals into existing parks and plazas where possible to encourage activity, security, and connectivity.
- Coordinate public spaces/ plazas with City of Dallas Parks Department and consider Downtown Parks Master Plan.



Excess land between rail tracks can be designed to be enjoyable plazas for pedestrians

Development Air Rights

- Maximize opportunities for developable parcels and allow development of air rights over DART right-of-way.
- Allow for development above light-rail train tunnel portals to maximize economic development, shielding tunnel portal from view while also discouraging devaluation of adjacent properties.



Allowing the development of air rights above the tracks can create great spaces atop would-be eyesores



This light rail in Portland runs beneath a building and through a plaza.



Air development rights produce increased economic development where otherwise no development could exist



Budget

- Consider a separate budget for programming and execution of corridor level public artwork to enhance the corridor as a whole. Coordinate all artwork with the City of Dallas' Office of Cultural Affairs.
- Seek out opportunities for public/ private partnerships for art programs or installations.

Opportunities

- Consider creating a specific Art Advisory Committee for each district corridor in addition to the Station Committee.
- Provide public art along corridors in addition to at stations.
- Follow DART Art and Design Criteria at stations.



Art at a transit stop and along the route provides enjoyment for waiting passengers and people passing by



This light rail station in Minneapolis incorporates art into the facade of a stairwell



This station in Portland has a striking art piece in the center of the station

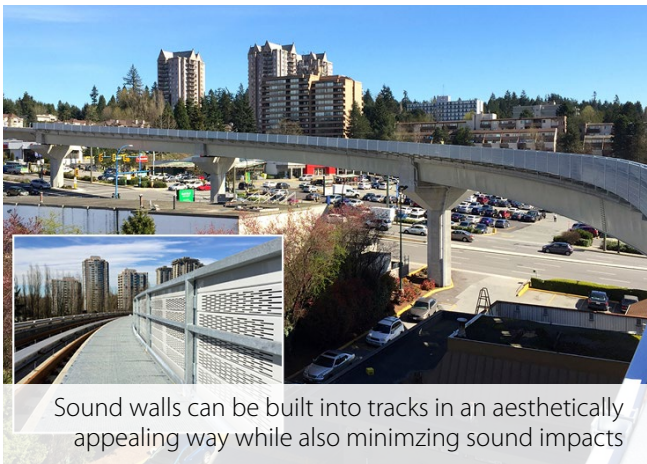


Artwork can be incorporated into the structure of the station platform, as seen here.



Sound Walls

- Provide context sensitive sound walls / landscape screening where needed to prevent visual intrusion of utilities and other elements .
- Address future development impacts as well as existing conditions where possible.



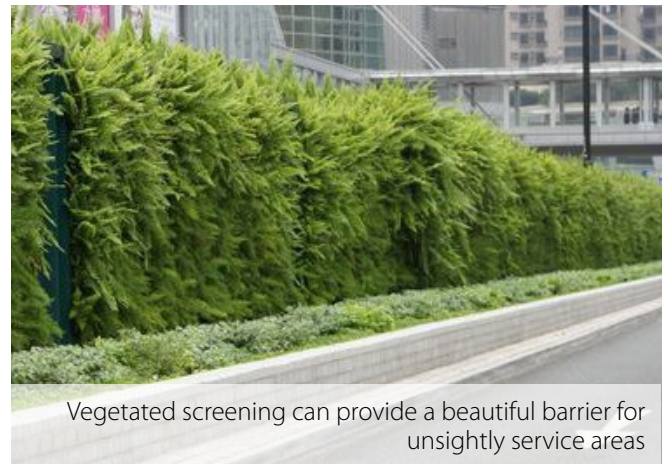
Sound walls can be built into tracks in an aesthetically appealing way while also minimizing sound impacts



These sound walls in Seattle help block sound from the train tracks from the surrounding neighborhood

Preventative Landscaping

- Provide landscape screening where needed to prevent visual intrusion of utilities and other elements .
- Provide landscaping & irrigation appropriate to the location (provide street trees at minimum 25' on center, where possible).
- Integrate landscaping into soundwalls, fencing, and screening elements.



Vegetated screening can provide a beautiful barrier for unsightly service areas



Vegetated buffers can also be designed with local plant species and serve to improve stormwater management



Safety and Security

- Coordinate pedestrian safety at crossings and along corridor with City of Dallas.
- Provide vehicle crossing protection that is fully integrated with traffic signal system in the corridor.
- Provide active surveillance in all areas that are not highly visible. Enhance surveillance in below-grade stations.
- Follow CPTED (Crime Prevention Through Environmental Design) Principles in all station and transitway design

CPTED Principle #1: Natural Surveillance--"See and be seen". Lighting and landscape play an important role. Minimize or eliminate physical obstructions that obscure views.

CPTED Principle #2: Natural Access Control-- Utilize walkways, fences, lighting, signage and landscape to clearly guide people and vehicles to and from the proper entrances.

CPTED Principle #3: Territorial Reinforcement-- Utilize pavement treatments, landscaping, signage, etc. to distinguish private and public areas.

CPTED Principle #4: Maintenance-- Neglected and poorly maintained properties are breeding grounds for criminal activity.

Durability + Maintenance

- Materials should be selected based upon durability and low maintenance requirements.
- Materials should incorporate integral color wherever possible.
- Materials should deter vandalism as outlined in DART Design Criteria.
- Long term maintenance agreements for all improvements are encouraged to clearly identify maintenance responsibility. These entities include but are not limited to organizations such as City of Dallas, Downtown Dallas Inc., and DART.



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Adjacent Development Considerations

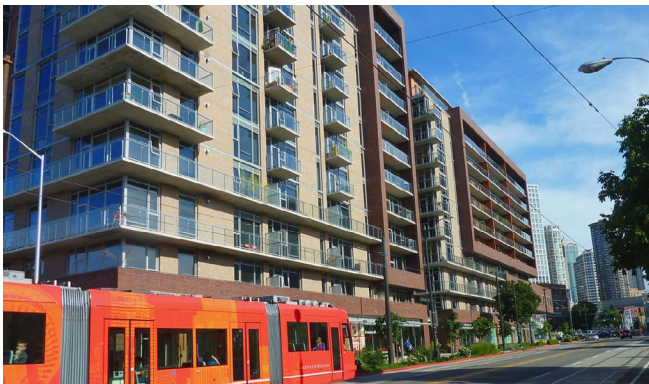
- 46 Development Character
- 46 Street and Block Design
- 47 Pedestrian Level Design
- 47 Sidewalk and Landscape Design



Development Character

Transit corridor design should:

- Allow for greater density near light rail stations.
- Not preclude the ability for adjacent development to maintain a consistent and continuous street wall with a pedestrian-oriented edge.
- Enhance the ability for adjacent development to collectively create spaces where pedestrians feel comfortable and protected rather than overwhelmed or vulnerable.
- Not preclude adjacent development from creating continuously lively and inviting street-level façades with storefronts, display windows, entrances, or other forms of architectural relief.
- Allow adjacent development to provide awnings and/or architectural detail.

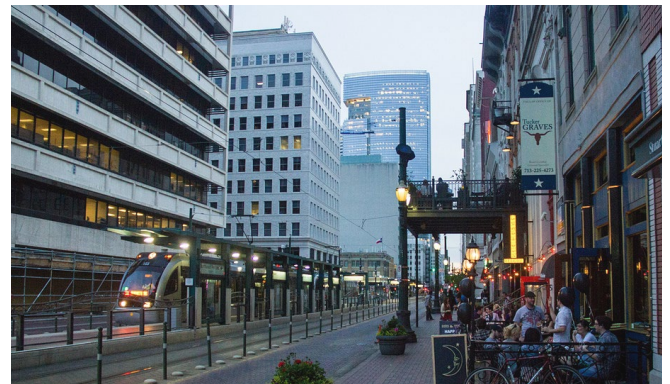


Transit-oriented development near light rail helps increase ridership and overall residential density

Street and Block Design

Transit corridor design should:

- Allow adjacent development to maintain or create short blocks with frequent intersections, avoiding super-blocks and multiple block developments that close streets.
- Allow for adjacent development to provide on-street parking where possible.
- Preserve important views.
- Not increase the number of curb cuts and vehicular access points to adjacent development.
- Consider local TIF district guidelines, including TIF district goals, character, and other special considerations, when designing DART lines through these particular geographies.



The light rail in Downtown Houston has an active public realm with numerous cafes with outdoor dining



Storefronts in Downtown Portland open out to the light rail very similar to streets with vehicular traffic



Providing alleyways, such as Linden Alley in San Francisco, increase connectivity on large sites



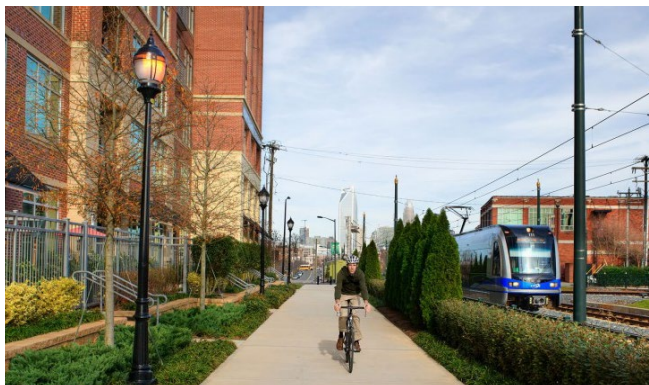
Pedestrian Level Design

Transit corridor design should:

- Not preclude the ability for adjacent development to provide raised or setback ground-level entries such as stoops and porches. Additionally, design shall not preclude adjacent buildings to provide prominent and accessible entrances connecting the private and public realms.
- Allow for a variety of signage at ground level including awning signage, projecting signage, window signs, blade signs, and temporary sandwich boards. Additionally, design shall not obscure sight lines of building entrances and signage.
- Not preclude adjacent development from providing sidewalk vendors, cafes, or restaurant patios adjacent to the curb where space permits.



Portland's light rail transit mall has a design that allows for numerous entrances to retail and commercial spaces



This development in Charlotte provides a trail and residential stoops that look onto the adjacent rail line

Sidewalk and Landscape Design

Transit corridor design should:

- Preserve significant trees within public right-of-way and on adjacent property.
- Allow for proper drainage and irrigation for street trees and trees on adjacent properties.
- Allow adjacent development to locate benches and seating near building entrances and in public realm away from street. Additionally, the design shall allow for non-permanent amenities such as seating and tables.
- Not preclude adjacent development to provide ample lighting on sidewalks, streets, walkways and plazas to enhance safety, including street lights spaced a maximum of 75' apart.



Wide sidewalks provide a large clear-zone as well as street trees and other street furniture



This light rail station in Charlotte has flexible furniture and cafe seating adjacent to private development



DOWNTOWN
DALLAS INC

Memorandum



CITY OF DALLAS

DATE February 24, 2017

The Honorable Members of the Transportation and Trinity River Project Committee:

TO Lee M. Kleinman (Chair), Deputy Mayor Pro Tem Erik Wilson (Vice-Chair), Mayor Pro Tem Monica R. Alonzo, Sandy Greyson, Adam Medrano, and Casey Thomas II

SUBJECT **Advanced Traffic Management System Project Update**

On Monday, February 27, 2017 you will be briefed on the Advanced Traffic Management System Project Update. The briefing materials are attached for your review.

Please feel free to contact me if you have any questions or concerns.

A handwritten signature in blue ink that reads "Jill Jordan".

Jill A. Jordan, P.E.,
Assistant City Manager

c: Honorable Mayor and Members of the City Council
T.C. Broadnax, City Manager
Larry Casto, City Attorney
Craig D. Kinton, City Auditor
Rosa A. Rios, City Secretary
Daniel F. Solis, Administrative Judge
Kimberly Bizzor Tolbert, Chief of Staff to the City Manager

Mark McDaniel, Acting First Assistant City Manager
Eric D. Campbell, Assistant City Manager
Joey Zapata, Assistant City Manager
M. Elizabeth Reich, Chief Financial Officer
Sana Syed, Public Information Officer
Elsa Cantu, Assistant to the City Manager – Mayor & Council
Directors and Assistant Directors

Advanced Traffic Management System Project Update

**Transportation and Trinity
River Project Committee
February 27, 2017**

**Auro Majumdar, Assistant Director
Mobility and Street Services Department
City of Dallas**



Presentation Overview

- Background/History
- Purpose
- Benefits of Traffic Management System
- Need for Upgrade
- Project Status
- Next Steps



Background

- City's 1980s era analog Traffic Management System, which controls all traffic signals, Dynamic Message signs and Video Cameras, is obsolete and not supported by manufacturers
- The Advanced Traffic Management System (ATMS) Project will replace the current obsolete system with a modern digital system

Previous Council Action

- May 2013 - Council approved purchase of Advanced Traffic Controllers (ATCs)
- September 2013 - Transportation and Trinity River Project Committee briefed on the ATMS project
- May 2014 - Council approved consultant contract to develop specifications for Central Computer System
- September 2014 - Council accepts \$3,847,094 NCTCOG grant for ATMS project
- June 2015 - Council accepts \$2,500,000 NCTCOG/TXDOT grant for the ATMS project

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Purpose

This briefing:

- Enumerates benefits of a traffic management system
- Explains the need for upgrading present system
- Provides an update on progress to date on the ATMS project

Benefits of Traffic Management System

- Maximizes progression, minimizes stops
- Reduces congestion, air pollution, fuel consumption and red light running
- Saves time and money – Efficient progression reduces loss of productive time stuck in traffic

Benefits of Traffic Management System contd..

- Signals timed remotely with minimal staff - faster, efficient operations
- Provides capability for incidence management during accidents/events
- Notifies staff about malfunction - reduces maintenance response time
- Benefits over 100 events annually at the American Airlines Center and Fair Park

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Traffic Signal System Six Components

Traffic Signal



Vehicle Detectors



Controller Cabinet



Central Computer System



Communication Link



Traffic Signal Controller



ATMS Upgrade Project - Three Components

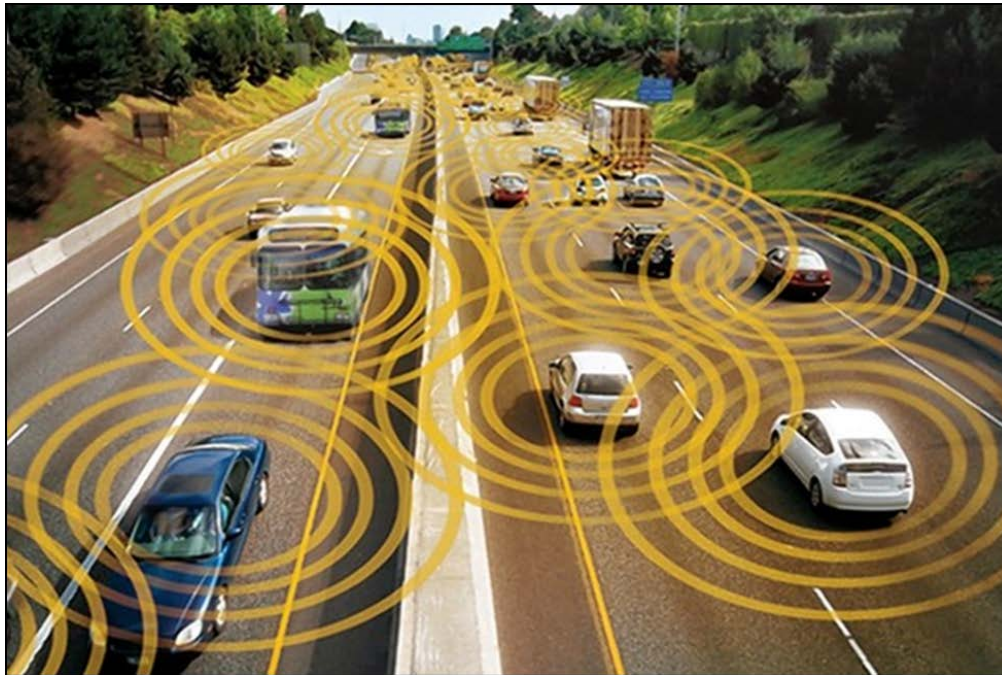
Traffic Signal Controller

- Computer that controls the traffic signal in the field
- Analyzes data from vehicle detectors and other components of the traffic signal
- Communicates with and reports problems to the traffic management center



Communication

- Connects traffic signal controller to central computer system



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Central Computer System

- Controls signal timing for the over 1,500 traffic signals in the system
- Can remotely change signal timing
- Can collect traffic data from individual signals
- Allows for remote programming of Dynamic message signs
- Manages traffic monitoring video cameras



Need for Upgrade

- Traffic signal controllers
- Installed in the early 1990's and are obsolete
- Controller software not supported by developer anymore
- Communication
- Obsolete analog communication over abandoned “twisted pair” Time Warner television cable
- Central computer system
- The 1980's era computer system is past its useful life
- Is not supported by the manufacturer anymore - cannot be repaired if it breaks down – parts are not available

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ATMS Upgrade Project

Replaces three components of the traffic signal system:

- Existing traffic signal controller with Advanced Traffic Signal Controller (ATC)
- Aging Central Computer System (CCS) with new, versatile CCS
- Existing analog communication network with high speed digital communications

Advanced Traffic Signal Controller (ATC)

Project History

- 2011 - Cities of Dallas, Richardson and Ft Worth jointly selected a consultant to develop specifications for ATC
- Procurement effort led by Ft. Worth - Intelight selected as preferred vendor
- 2013 - Council approved ATC procurement

ATC – Project Status

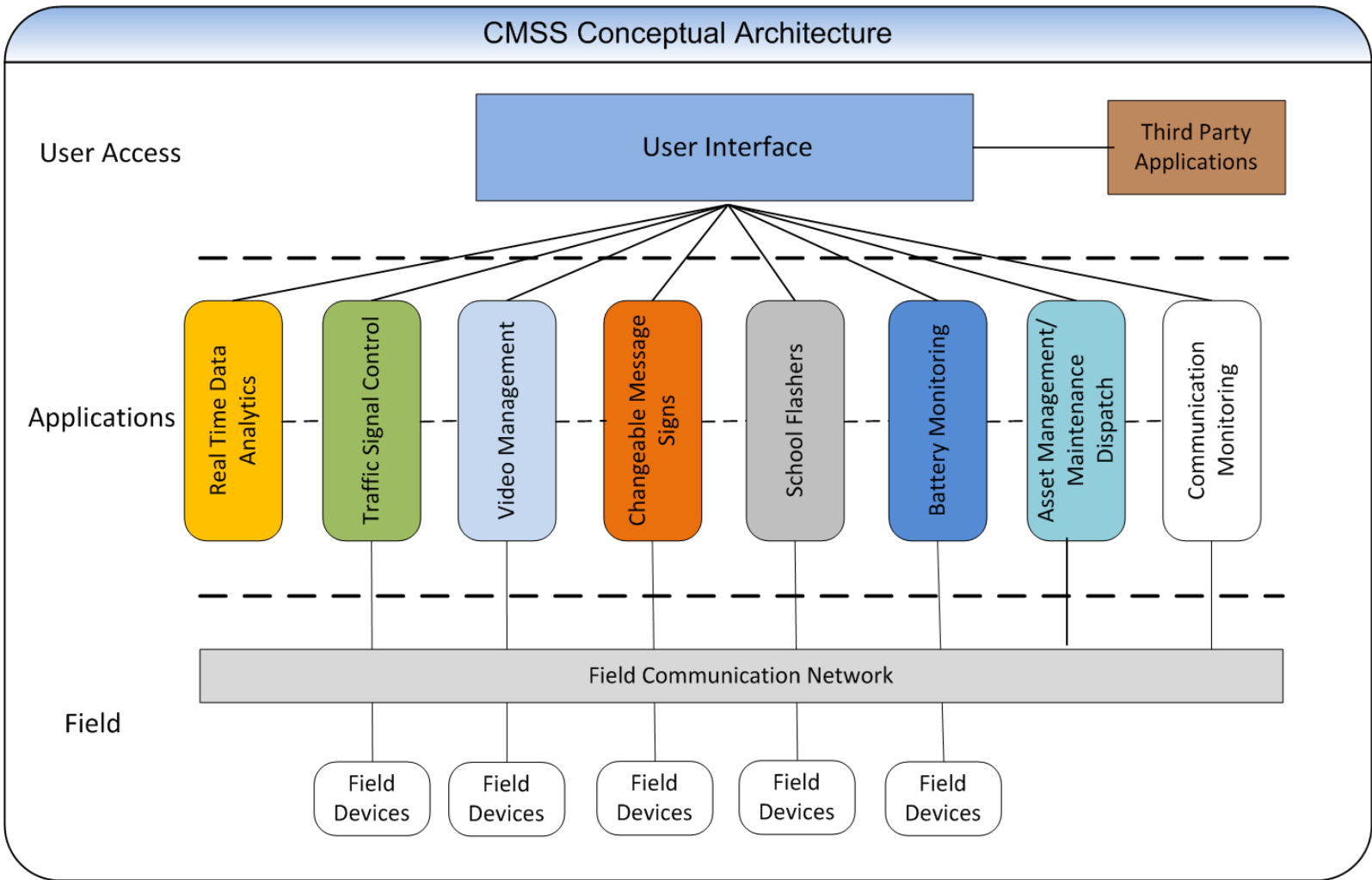
- Deployment of ATCs is critical path item for ATMS project – controllers for 1500+ traffic signals have to be individually programmed, deployed and field tested
- Software testing and acceptance is substantially complete
- Staff has been steadily deploying ATCs over the past two years - over 440 ATCs have been deployed to date

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CCS – Project Status

- Specifications development complete
- System Integrator to develop GUI and provide applications solution selected
- Negotiations on-going with the System Integrator
- Council award in summer 2017

CMSS Conceptual Architecture



Communications System

- CIS testing various communication options citywide
- Options include cellular modems, radio, fiber and mesh
- Cellular modems are the most viable option for traffic signals and is being used in the new ATCs
- All ATCs deployed are using modems – over 440 have been installed to date

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ATMS Upgrade Project Highlights

Truly “Regional” project

- Specifications and requirements developed jointly by traffic staff of the cities of Dallas, Fort Worth, Richardson and Irving. Due to regional benefits of project, Dallas received \$6.3M grants from NCTCOG. Partner cities received additional grant funds
- Will result in uniformity of signal operations for the major cities in DFW region and provide for progression along corridors across jurisdictions
- Will provide for remote and inter-jurisdictional operations capabilities during emergencies and natural disasters.
- Will meet Homeland Security standards

ATMS Upgrade Project Highlights

Versatile and Economical

- Modular Architecture - will allow for future expansion and integration of individual components without wholesale system replacement. Uses off the shelf hardware with expandable software capabilities
- Adaptable and economical – Capable of integrating multiple systems. More economical than newer systems in other major cities

Next Steps

- System Integrator Contract to Council – Summer 2017
- Basic Interim Traffic Signal Management system established – December 2017
- Final modules for critical systems established - 2019
- ATC deployment complete - 2020
- Begin deploying other modules with Council approval/funding

QUESTIONS?



Advanced Traffic Management System Project Update

**Transportation and Trinity
River Project Committee
February 27, 2017**

**Auro Majumdar, Assistant Director
Mobility and Street Services Department
City of Dallas**



Interagency Transportation Report RTC / TxDOT / NTTA / DART / DRMC / DFW Airport / HSR February 2017

Regional Transportation Council (RTC)

Federal Funding Available to Expand Public Transit Options. The RTC announced the availability of \$5.5 million for two federal programs – Enhanced Mobility of Seniors and Individuals with Disabilities and Job Access/Reverse Commute Projects. The “Enhanced Mobility” Program provides specialized transportation for seniors or the disabled when public transportation is insufficient or unavailable. The “Job Access” Program provides funds to support projects that transport individuals with low incomes to/from employment activities. Pre-proposal workshops will be held on March 8th and 10th to explain eligibility requirements, application process, selection criteria and timeline. The deadline to submit a proposal is April 7th.

People Mover Workshop. NCTCOG hosted a People Mover System Workshop on June 27, 2014 to gauge interest in the use of automated people mover systems in the region and to identify potential activity centers that could be effectively served by the technology. At the time, the type of people mover envisioned would be like the Skylink system at DFW Airport. The Medical District area, Love Field, and the Midtown development areas were identified as potential locations in Dallas. Prior to their regular business meeting on February 9th, the RTC held a workshop to cast a vision for a new kind of “modern” people mover that could take advantage of advances in automated vehicle technology to design a system that would be less expensive to construct and operate, as well as provide more flexible operations. NCTCOG staff is working with the City of Dallas to issue a People Mover RFP to begin planning and preliminary engineering for the Midtown development area.

Preparation of 2018-19 Unified Planning Work Program (UPWP). The UPWP is the federally-required work program for the transportation planning staff at NCTCOG. The document is adopted in full every two years and amended from time to time. The emphasis of the UPWP is on regional transportation and related air quality planning activities. A portion of the UPWP identifies tasks to assist local governments and transportation agencies with technical assistance for planning. NCTCOG staff has begun preparation of UPWP for fiscal years 2018 and 2019. They have asked area agencies to identify transportation planning assistance needs and submit them by March 24th.

Texas Department of Transportation (TxDOT)

Update on IH 635 LBJ East Public Hearing. TxDOT’s public hearing for the LBJ East project (from US 75 to IH 30), held on January 31st, showed strong public support for the proposed improvements, including the use of tolled managed lanes between US 75 and Royal/Miller. The Dallas City Council’s resolution of support for the project approved on February 8th, was delivered to the TxDOT Dallas District Office for inclusion in the official public hearing record.

Texas Freight Mobility Plan. In response to requirements in the federal FAST Act, TxDOT is updating their statewide freight plan which was originally adopted in January 2016. TxDOT staff and consultants are conducting workshops around the state to gather input from the freight industry, local governments and citizens. The DFW workshop was held on February 8th at the NCTCOG offices. One aspect of the update is to recommend additional highway segments to

be identified on the National Freight Network; this would make them eligible to receive federal funding for freight-related improvements. Virtually all the limited-access highways in the City of Dallas are already included on the National Freight Network. The updated plan must be approved by the Federal Highway Administration by December 4th.

North Texas Tollway Authority (NTTA)

DNT Resurfacing and Center Barrier Replacement. The NTTA will begin an improvement project on the Dallas North Tollway (DNT) between Harvest Hill Road (near LBJ) and IH 35E in April. The two-year project will include replacing the tollway's center barrier and pavement. To minimize traffic impacts, the improvement work will be conducted during off peak hours at night and during some weekends. Some temporary lane and ramp closures may be needed. NTTA staff will be coordinating with affected Council members to help keep citizens informed.

Dallas Area Rapid Transit (DART)

2040 Transit System Plan Briefing. The full DART Board was briefed February 14th on the status of the 2040 Transit System Plan. The latest work has included an evaluation of nine candidate corridors for new or extended rail transit service. DART staff identified two corridors for further study – a West Dallas line from downtown to Loop 12 and an LBJ/Inwood line stretching from Northwest Highway in Garland, west to the Park Central and Midtown areas before connecting to the Cotton Belt near the DNT. Since the Transit System Plan is updated every five years, DART staff is recommending that the agency focus on bus service plan enhancements, implementation of the D2 Subway, Cotton Belt, and Streetcar Central Link projects, and further study of the West Dallas and LBJ corridors. The Board is expected to consider approval of the 2040 Plan by late spring.

DART Begins Review of Fare Structure. The DART Board's Budget and Finance Committee was briefed on January 24th on a process to review and update its fare structure. DART reviews its fares approximately every five years. DART staff will develop recommendations and conduct an equity analysis for Board consideration. Public meetings will be scheduled in June, and Board approval is anticipated by August 2017. Fare increases are likely to be phased in over a 12-month period.

Dallas Regional Mobility Coalition (DRMC)

Harvey Kronberg Featured Speaker. The February 3rd DRMC meeting featured Harvey Kronberg, publisher and editor of the Quorum Report (QR). The QR is a non-partisan newsletter focusing on Texas politics and government. It was founded in 1983 and is the oldest political newsletter in Texas. Mr. Kronberg has been the Quorum's publisher since 1989.

Dallas-Fort Worth International Airport (DFW Airport)

Airport Board Elects New Officers. At their February 2nd meeting, Matrice Ellis-Kirk was sworn in as the newest City of Dallas appointee to the DFW Airport Board, replacing Curtis Ransom. Following the swearing-in, Mayors Mike Rawlings and Betsy Price nominated the slate of officers to include: Sam Coats for Board Chair, Bill Meadows for Vice Chair, and Regina Montoya for Secretary. All three were unanimously approved by the Board.

TEXRail Station Construction. The Airport Board approved a resolution on February 2nd authorizing the CEO to execute a contract providing for the construction of the TEXRail Station

at Terminal B. TEXRail is expected to be completed by late 2018 and will provide direct commuter rail service from downtown Fort Worth to DFW Airport.

DFW to Add Refrigeration Facilities to Serve Cargo Customers. Cargo and logistics customers will soon have better opportunities to ship perishable items through one of the world's premier international gateways. DFW Airport will install a cold chain facility that will be operated by AirLogistix USA. Expected to be operational this summer, the state-of-the-art transfer facility will give DFW the ability to precisely control warehousing temperatures for shipments of pharmaceuticals, flowers and fresh foods.

High-Speed Rail (HSR)

RTC Requests Federal Funding Support for High-Speed Rail. The RTC approved a letter to new USDOT Secretary Elaine Chao requesting \$2 billion from the USDOT and Federal Railroad Administration (FRA) to support construction of a high-speed rail line between Dallas and Fort Worth. This level of funding is the anticipated gap funding that the public sector would need to contribute to attract a private sector partner to construct, operate and maintain this HSR link.

KEY FOCUS AREA: Public Safety
AGENDA DATE: March 22, 2017
COUNCIL DISTRICT(S): 3
DEPARTMENT: Aviation
CMO: Mark McDaniel, 670-3256
MAPSCO: 63L

SUBJECT

Authorize **(1)** the Aviation Department to provide initial funding for maintenance costs for the Routine Annual Maintenance Program (RAMP) at Dallas Executive Airport in an amount not to exceed \$100,000; **(2)** the Aviation Department to apply for and accept grant funds from the Texas Department of Transportation Aviation Division in an amount not to exceed \$50,000 for eligible costs related to RAMP as part of a 50% conditional grant match agreement; and **(3)** execution of the grant agreement - Total not to exceed \$100,000 - Financing: Aviation Current Funds (\$100,000) (subject to appropriations; to be partially reimbursed with TxDOT Grant Funds) and Texas Department of Transportation Grant Funds (\$50,000)

BACKGROUND

The Routine Airport Maintenance Project (RAMP) is a reimbursement that will enable the Department of Aviation to conduct needed repairs on the airport for annual maintenance. Some of the examples of these programs in the Ramp include Runway crack seal and Ramp area crack seal. The primary objective of the project is:

- Conduct repairs as needed under the Routine Airport Maintenance Program under the guidelines of Texas Department of Transportation (TxDOT).

TxDOT will determine that the repairs are within the scope of eligible projects for the TxDOT Grant for Routine Airport Maintenance Program Funds and will participate in an amount equal to 50% of the eligible project costs, TxDOT reimbursement not to exceed \$50,000.00.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized grant reimbursement for the RAMP projects for fiscal year 2015, on May 13, 2015, by Resolution No. 15-0834.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS) (Continued)

Authorized grant reimbursement for the RAMP projects for fiscal year 2016, on May 25, 2016, by Resolution No. 16-0794.

Information about this item will be provided to the Transportation and Trinity River Project Committee on February 27, 2017.

FISCAL INFORMATION

\$100,000.00 - Aviation Current Funds (subject to annual appropriations; subject to reimbursement of up to \$50,000.00 from TxDOT)

\$50,000.00 - TxDOT Routine Airport Maintenance Program Grant Funds

March 22, 2017

WHEREAS, the City of Dallas intends to enhance the city's economic vibrancy; particularly within the city's southern sector; and,

WHEREAS, Dallas Executive Airport has proven to be a significant economic generator within the city's southern sector; and,

WHEREAS, it is desirable to maintain the airport infrastructure to provide a safe and attractive environment for aircraft owners and operators to conduct business at Dallas Executive Airport; and,

WHEREAS, the City of Dallas & Texas Department of Transportation (TxDOT) participated in the RAMP program for fiscal year 2016 and accept reimbursement funds from TxDOT for eligible costs for routine airport maintenance at Dallas Executive Airport per the agreement; and,

WHEREAS, the total maintenance cost estimated for fiscal year 2017 agreement to be up to \$100,000.00 and the City of Dallas will be responsible for the total project costs with a reimbursement by TxDOT of 50% of the total project cost up to and not to exceed \$50,000.00; and,

WHEREAS, that TxDOT has indicated the routine airport maintenance is a program that is eligible for state funding through their Grant for Routine Airport Maintenance Program (RAMP) and will participate in an amount equal to 50% of the eligible costs, not to exceed \$50,000.00.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to enter into an agreement with TxDOT for the Routine Annual Maintenance Program for fiscal year 2017 in an amount not to exceed \$100,000.00; the Aviation Department is hereby authorized to apply for and accept grant funds for fiscal year 2017 from the Texas department of Transportation Aviation Division in an amount not to exceed \$50,000.00 for eligible costs related to Ramp as part of a 50% conditional grant match agreement; and to execute the grant agreement subject to the approval of the City Attorney.

Section 2. That upon acceptance of annual TxDOT RAMP reimbursement funding by the Chief Financial Officer, funds will be placed in the Aviation Grant Fund S278, Dept. AVI, Unit 1747, Revenue Source 6526.

Section 3. That the City Manager is hereby authorized to increase appropriations in the Aviation Grant Fund S278, Dept. AVI, Unit 1747, Object Code 3099 for the amount that is approved by TxDOT for reimbursement for fiscal year 2017.

March 22, 2017

Section 4. That the Chief Financial Officer is hereby authorized to reimburse Aviation Operating Fund 0130, Dept. AVI, Unit 7731, Object 5011 for the amount that is approved by TxDOT Routine Airport Maintenance Program (RAMP) for annual reimbursement from the Aviation Grant Fund S278 Dept. AVI, Unit 1747, Object 3099 for the amount that is approved by TxDOT for annual reimbursement not to exceed \$50,000 annually.

Section 5. That the City Manager is hereby authorized to reimburse the granting agency any expenditure identified as ineligible and notify the appropriate City Council Committee of expenditures identified as ineligible not later than 30 days after the reimbursement.

Section 6. That the City Manager shall keep the appropriate City Council Committee informed of all final granting agency monitoring reports not later than thirty (30) days after the receipt of the report.

Section 7. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: March 22, 2017
COUNCIL DISTRICT(S): 2
DEPARTMENT: Aviation
CMO: Mark McDaniel, 670-3256
MAPSCO: 34E

SUBJECT

Authorize Supplemental Agreement No. 4 to the professional services contract with Garver, LLC for design, planning, engineering, and construction document services for the Love Field Pavement Rehabilitation and Reconstruction Project, Taxiway Bravo Phase II, and Taxiway Mike Phase II at Dallas Love Field - Not to exceed \$1,737,770, from \$3,456,534 to \$5,194,304 - Financing: Aviation Capital Construction Funds (\$698,258), Aviation Near Term Passenger Facility Charge Funds (\$250,000) and Federal Aviation Administration Airport Improvement Program Grant Funds (\$789,512)

BACKGROUND

This action will authorize Supplemental Agreement No. 4 to the professional services contract with Garver, LLC for design, planning, engineering, and construction document services for the Love Field Pavement Rehabilitation and Reconstruction Project, Phase II.

The Program's 2015 fiscal year Airfield Pavement Rehabilitation and Reconstruction Project includes the reconstruction of a portion of Taxiway Bravo and new construction of a portion of Taxiway Mike Phase II, which requires engineering design services.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized a professional services contract with Garver, LLC to provide design and preparation of construction documents for the Airfield Pavement Repairs on March 25, 2015, by Resolution No. 15-0491.

Authorized Supplemental Agreement No. 2 to the professional services contract with Garver, LLC for construction administration and management for the Airfield Pavement Repairs at Dallas Love Field on September 14, 2016, by Resolution No. 16-1512.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS) (Continued)

Authorized Supplemental Agreement No. 3 to the professional services contract with Garver, LLC for on-site construction administration and construction observation services for the Taxiway Echo Intersection Improvements and the Taxiway Bravo projects at Dallas Love Field on September 28, 2016, by Resolution No. 16-1581.

Authorized acceptance of a grant and deposit of reimbursement funds from the Federal Aviation Administration (FAA) (Grant No. 3-48-0062-51-2016/CFDA No. 20.016) for Rehabilitation of Taxiway B from RWY 13L to 18-36 project to the Aviation Airport Improvement Program Grant Fund F491, on November 9, 2016, by Resolution No. 16-1796.

Information about this item will be provided to the Transportation and Trinity River Project Committee on February 27, 2017.

FISCAL INFORMATION

\$698,258.00 - Aviation Capital Construction Funds
\$250,000.00 - Aviation Near Term Passenger Facility Charge Funds
\$789,512.00 - Federal Aviation Administration Airport Improvement Program Grant Funds

Design and construction documents	\$1,323,890.00
Supplemental Agreement No. 1	\$ 48,840.00
Supplemental Agreement No. 2	\$ 314,464.00
Supplemental Agreement No. 3	\$1,769,340.00
Supplemental Agreement No. 4 (this action)	<u>\$1,737,770.00</u>
Total	\$5,194,304.00

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

Garver, LLC

Hispanic Female	1	Hispanic Male	8
African-American Female	3	African-American Male	6
Other Female	7	Other Male	8
White Female	94	White Male	308

OWNER

Garver, LLC

Daniel H. Williams, President
Brock E. Hoskins, Senior Vice President
Herbert J. Parker, Senior Vice President

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize Supplemental Agreement No. 4 to the professional services contract with Garver, LLC for design, planning, engineering, and construction document services for the Love Field Pavement Rehabilitation and Reconstruction Project, Taxiway Bravo Phase II, and Taxiway Mike Phase II at Dallas Love Field - Not to exceed \$1,737,770, from \$3,456,534 to \$5,194,304 - Financing: Aviation Capital Construction Funds (\$698,258), Aviation Near Term Passenger Facility Charge Funds (\$250,000) and Federal Aviation Administration Airport Improvement Program Grant Funds (\$789,512)

Garver, LLC is a local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Professional Services

LOCAL/NON-LOCAL CONTRACT SUMMARY - THIS ACTION ONLY

	<u>Amount</u>	<u>Percent</u>
Local contracts	\$1,672,970.00	96.27%
Non-local contracts	\$64,800.00	3.73%
TOTAL THIS ACTION	\$1,737,770.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION THIS ACTION

Local Contractors / Sub-Contractors

<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
EJES, Inc	BMDB53984Y1017	\$197,986.28	11.83%
STL Engineers	HFDB85904Y0617	\$73,820.00	4.41%
Arredondo, Zepeda & Brunz, LLC	HMDB34314Y0617	\$130,000.00	7.77%
Total Minority - Local		\$401,806.28	24.02%

Non-Local Contractors / Sub-Contractors

<u>Non-local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Williams CM Group	WFDB16721Y0717	\$64,800.00	100.00%
Total Minority - Non-local		\$64,800.00	100.00%

TOTAL M/WBE PARTICIPATION

	This Action		Participation to Date	
	<u>Amount</u>	<u>Percent</u>	<u>Amount</u>	<u>Percent</u>
African American	\$197,986.28	11.39%	\$356,860.07	6.87%
Hispanic American	\$203,820.00	11.73%	\$412,560.71	7.94%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$64,800.00	3.73%	\$1,070,212.50	20.60%
Total	<u>\$466,606.28</u>	<u>26.85%</u>	<u>\$1,839,633.28</u>	<u>35.42%</u>

March 22, 2017

WHEREAS, on March 25, 2015, Resolution No. 15-0491 authorized a professional services contract with Garver, LLC to provide design and preparation of construction documents for the Airfield pavement repairs in the amount of \$1,323,890.00; and,

WHEREAS, on October 23, 2015, Administrative Action No. 15-7086 authorized Supplemental Agreement No. 1 to the professional services contract with Garver, LLC to complete a field investigation and prepare construction documents for isolated full-depth concrete repairs on the airfield at Dallas Love Field in the amount of \$48,840.00, from \$1,323,890.00 to \$1,372,730.00; and,

WHEREAS, on September 14, 2016, Resolution No. 16-1512 authorized Supplemental Agreement No. 2 to the professional services contract with Garver, LLC for construction administration and management for the Airfield Pavement Repairs at Dallas Love Field in the amount of \$314,464.00, from \$1,372,730.00 to \$1,687,194.00; and,

WHEREAS, on September 28, 2016, Resolution No. 16-1581 authorized Supplemental Agreement No. 3 to the professional services contract with Garver, LLC for on-site construction administration and construction observation services for the Taxiway Echo Intersection Improvements and the Taxiway Bravo projects in the amount of \$1,769,340.00, increasing the amount from \$1,687,194.00 to \$3,456,534.00; and,

WHEREAS, on November 9, 2016, Resolution No. 16-1796 authorized the acceptance of a grant and deposit of reimbursement funds from the Federal Aviation Administration (FAA) (Grant No. 3-48-0062-51-2016/CFDA No. 20.016) for Rehabilitation of Taxiway B from RWY 13L to 18-36 project to the Aviation Airport Improvement Program Grant Fund F491 in the amount of \$789,512.00; and,

WHEREAS, it is now necessary to execute Supplemental Agreement No. 4 to the professional services contract with Garver, LLC to provide engineering planning services for the Love Field Pavement Rehabilitation and Reconstruction Project, Taxiway Bravo Phase II and Taxiway Mike Phase II at Dallas Love Field in an amount not to exceed \$1,737,770.00, increasing the amount from \$3,456,534.00 to \$5,194,304.00.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

March 22, 2017

Section 1. That the City Manager is hereby authorized to execute Supplemental Agreement No. 4 to the professional services contract with Garver, LLC to provide design, planning, engineering, and construction document services for the Love Field Pavement Rehabilitation and Reconstruction Project, Taxiway Bravo Phase II, and Taxiway Mike Phase II, in an amount not to exceed \$1,737,770.00, increasing the amount from \$3,456,534.00 to \$5,194,304.00.

Section 2. That the City will apply for grant funds from the Federal Aviation Administration (FAA) at a later date for Taxiway Mike Phase II Project and upon approval and deposit of the grant funds, the Chief Financial Officer is authorized to reclassify eligible design, planning, engineering, and construction document costs to the Aviation Grant Fund F515, Dept: AVI, Unit W153, OBJ 4111 from Aviation Capital Construction Fund 0131, Department AVI, Unit W153, OBJ 4111 in an amount not to exceed \$698,258.00.

Section 3. That the City Manager is hereby authorized to establish appropriations in the Aviation Grant Fund F491, Dept. AVI, Unit W121, Object 4111 in an amount not to exceed \$789,512.00 for the Taxiway B from RWY 13L to 18-36 Project.

Section 4. That the City Manager is hereby authorized to establish appropriations in the Aviation Near Term Passenger Facility Charge Fund A477, Dept. AVI, Unit W121, Object Code 3099 in an amount not to exceed \$250,000.00 for the Taxiway Bravo from RWY 13L to 18-36 project.

Section 5. That the City will apply for future Passenger Facility Charge (PFC) funding for the eligible design, planning and construction document costs for the Taxiway B from RWY 13L to 18-36 project; and upon the approval of PFC by the FAA for this project, the City Manager is authorized to increase appropriations of the Passenger Facility Charge Fund 0477, Dept. AVI, Unit 0781, Object Code 3099 by \$250,000.00 and transfer to the Aviation Near Term Passenger Facility Charge Fund A477, Dept. AVI, Unit W121, Object Code 5011.

Section 6. That the Chief Financial Officer is hereby authorized to disburse funds in an amount not to exceed \$1,737,770.00 to be paid to Garver, LLC in accordance with the terms and conditions of the contract, as follows:

Taxiway Mike Phase II Project

Aviation Capital Construction Fund
Fund 0131, Dept. AVI, Unit W153, Act. AAIP, Object Code 4111
Program: AVIW153, Encumbrance No. CTAVIW153GARFY17,
Commodity Code 92500, Vendor No. VS0000016343,
in an amount not to exceed:

\$ 698,258.00

March 22, 2017

Rehabilitate Taxiway Bravo from RWY 13L to 18-36 Project

Aviation Grant Fund No. 3-48-0062-51-2016/CFDA No. 20.16
Fund F491, Dept. AVI, Unit W121, Act. AAIP, Object Code 4111
Program: AVIW121, Encumbrance No. CTAVIW121TWYBFY17,
Commodity Code 92500, Vendor No. VS0000016343,
in an amount not to exceed: \$ 789,512.00

Aviation Near Term Passenger Facility Charge Fund

Fund A477, Dept. AVI, Unit W121, Act. AAIP, Object Code 4111
Program: AVIW121, Encumbrance No. CTAVIW121TWYBFY17,
Commodity Code 92500, Vendor No. VS0000016343,
in an amount not to exceed: \$ 250,000.00

Total \$1,737,770.00

Section 7. That the City Manager is hereby authorized to reimburse to the granting agency expenditures identified as ineligible. The City Manager shall notify the appropriate City Council Committee of expenditures identified as ineligible not later than 30 days after the reimbursement.

Section 8. That the City Manager shall keep the appropriate City Council Committee informed of all final granting agency monitoring reports not later than 30 days after the receipt of the report.

Section 9. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: March 22, 2017
COUNCIL DISTRICT(S): Outside City Limits
DEPARTMENT: Water Utilities
CMO: Mark McDaniel, 670-3256
MAPSCO: Outside City Limits

SUBJECT

Authorize an amendment to an Interlocal Agreement between the City of Dallas, City of Grapevine and the Dallas County Park Cities Municipal Utility District, to extend the term of the contract by ten-years, with four ten-year extension options, for continuation of the reimbursement to the City of Grapevine for Dallas' cost share for the administration of the Lake Grapevine Reservoir Accounting Plan - Not to exceed \$60,000 - Financing: Water Utilities Current Funds (subject to annual appropriations)

BACKGROUND

The City of Dallas, City of Grapevine and the Dallas County Park Cities Municipal Utility District (District) each owns a water right authorizing storage in, and diversion from the Lake Grapevine Reservoir. In the late 1980s, the City of Dallas and the City of Grapevine initiated litigation against the Dallas County Park Cities Municipal Utility District, disputing water rights in Lake Grapevine Reservoir. The dispute was ongoing and was resolved in 2001 through a mutual agreement between all parties. As terms of the settlement, the Texas Commission on Environmental Quality ruled in the best interest of all parties involved that a reservoir accounting plan must be implemented.

In 2001, the City of Dallas, City of Grapevine and the District entered into a settlement agreement to resolve their previous conflicts. In 2003, the City of Dallas, City of Grapevine and the District entered into an Interlocal Agreement for a Lake Grapevine Reservoir Accounting Plan to monitor the use of water from Lake Grapevine Reservoir. The cost of the maintenance of the accounting plan is shared equally between the City of Dallas, City of Grapevine and the District. After the expiration of the 2003 Interlocal Agreement, a new agreement was executed in 2013. The 2013 Interlocal Agreement has expired; however, the accounting is required to continue. The City of Grapevine has proposed an amendment to the 2013 Interlocal Agreement, to extend its term to 10 years, with four additional 10 year extensions, by mutual consent.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSION)

Council authorized settlement agreement between the City of Dallas, City of Grapevine and the District on June 27, 2001, by Resolution No. 01-2110.

Council authorized an agreement between the City of Dallas, City of Grapevine and Dallas County Park Cities Municipal Utility District to implement the Lake Grapevine Reservoir Accounting Plan and to reimburse the City of Grapevine for the City of Dallas' cost share of the agreement on September 10, 2003, by Resolution No. 03-2467.

Council authorized an Interlocal Agreement between the City of Dallas, City of Grapevine and the Dallas County Park Cities Municipal Utility District for continuation of the reimbursement to the City of Grapevine for Dallas' cost share for the administration of the Lake Grapevine Reservoir Accounting Plan on May 22, 2013, by Resolution No. 13-0856.

Information about this item was provided to the Transportation and Trinity River Project Committee on February 27, 2017.

FISCAL INFORMATION

\$60,000.00 – Water Utilities Current Funds (subject for annual appropriations)

March 22, 2017

WHEREAS, the City of Dallas, City of Grapevine and the Dallas County Park Cities Municipal Utility District are holders of water rights in Lake Grapevine Reservoir; and,

WHEREAS, the Texas Commission on Environmental Quality has approved a Lake Grapevine Reservoir Accounting Plan, which provides for the apportionment of the Lake Grapevine water supply between the City of Dallas, City of Grapevine and the Dallas County Park Cities Municipal Utility District; and,

WHEREAS, the City of Dallas, City of Grapevine and the Dallas County Park Cities Municipal Utility District desire to enter into an amendment to the Interlocal Agreement, to continue the administration of the Lake Grapevine Reservoir Accounting Plan.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is authorized to amend the Interlocal Agreement between the City of Dallas, City of Grapevine and the Dallas County Park Cities Municipal Utility District, to extend the term of the contract by ten-years, with four ten-year extension options, for continuation of the reimbursement to the City of Grapevine for Dallas' cost share for administration of the Lake Grapevine Reservoir.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in an amount not to exceed \$60,000 from the Water Utilities Current Fund subject to annual appropriations as follows:

<u>FUND</u>	<u>DEPT</u>	<u>UNIT</u>	<u>OBJ</u>	<u>ENCUMBRANCE</u>	<u>VENDOR</u>
0100	DWU	7030	3070	MASCCTDWU7030E1717	264280

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: March 22, 2017
COUNCIL DISTRICT(S): 5, 7, 10, 12
DEPARTMENT: Mobility and Street Services
CMO: Jill A. Jordan, P.E., 670-5299
MAPSCO: 17G Y 28E 57B 654Z

SUBJECT

Authorize **(1)** an Interlocal Agreement with the Texas Department of Transportation to provide cost reimbursement for material and labor costs incurred by the City for intersection and traffic signal improvements at the following five off-system locations: Marsh Lane and Rosemeade Parkway, Bruton Road and Second Avenue, Forest Lane and Audelia Road, Plano Road and Miller Road and Royal Lane and Skillman Road; **(2)** the receipt and deposit of funds from the Texas Department of Transportation in the amount of \$101,280; and **(3)** the establishment of appropriations in the amount of \$101,280 in the Texas Department of Transportation Grant Fund - Not to exceed \$101,280 - Financing: Texas Department of Transportation Grant Funds

BACKGROUND

The City of Dallas was awarded grant funding totaling \$6.17 million from the 2014 Highway Safety Improvement Program (HSIP) administered by the Texas Department of Transportation (TxDOT) to upgrade several high-accident intersections in the City of Dallas.

TxDOT will begin construction at five (5) of the above intersections: Marsh Lane and Rosemeade Parkway, Bruton Road and Second Avenue, Forest Lane and Audelia Road, Plano Road and Miller Road, and Royal Lane and Skillman Road. The intersection improvements at the five off-system intersections will include upgrades to pedestrian features, signage, pavement markings and reconstruction of traffic signals.

To remain consistent with other equipment in the city-wide system, the City of Dallas will provide labor and material for installation of traffic signal controllers, controller cabinets, traffic signs and other traffic control equipment for the project. TxDOT has agreed to reimburse the City for 100% of the above costs which includes labor and materials.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Information about this item was provided to the Transportation and Trinity River Project Committee on February 27, 2017.

ESTIMATED SCHEDULE OF PROJECT

Began Design December 2015
Complete Construction October 2019

FISCAL INFORMATION

Texas Department of Transportation Grant Funds - \$101,279.40

<u>Council District</u>	<u>Amount</u>
5	\$ 10,127.94
7	\$ 10,127.94
10	\$ 60,767.64
12	<u>\$ 20,255.88</u>
Total	\$101,279.40

MAP

Attached

March 22, 2017

WHEREAS, the Texas Department of Transportation (TxDOT) will provide reimbursement to the City of Dallas for material and labor costs incurred for improvements at five intersections, including upgrades to the traffic signals, specifically at Marsh Lane and Rosemeade Parkway, Bruton Road and Second Avenue, Forest Lane and Audelia Road, Plano Road and Miller Road, and Royal Lane and Skillman Road; and,

WHEREAS, TxDOT will reimburse 100% of labor and material costs in an amount not to exceed \$101,279.40; and,

WHEREAS, the City of Dallas desires to enter into an agreement with TxDOT to receive cost reimbursement for material and labor costs incurred for traffic signal improvements at Marsh Lane and Rosemeade Parkway, Bruton Road and Second Avenue, Forest Lane and Audelia Road, Plano Road and Miller Road, and Royal Lane and Skillman Road.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to execute an Interlocal Agreement with the Texas Department of Transportation (TxDOT) through a grant from the Department of Transportation Federal Highway Administration (CFDA 20.205) Agreement numbers CSJ #0918-46-280 (Marsh Lane and Rosemeade Parkway), CSJ #0918-47-121 (Bruton Road and Second Avenue), CSJ #0918-47-122 (Forest Lane and Audelia Road), CSJ #0918-47-126 (Plano Road and Miller Road) and CSJ #0918-47-129 (Royal Lane and Skillman Road), after it has been approved as to form by the City Attorney. The agreement remains effective as long as the project is incomplete or unless otherwise terminated or modified.

Section 2. That the Chief Financial Officer is hereby authorized to receive and deposit all reimbursements from TxDOT pertaining to this project in an amount not to exceed \$101,279.40 in Fund F8FT, Dept. STS, Unit 88FT, Revenue Source 6506.

Section 3. That the City Manager is hereby authorized to establish an appropriation in the amount of \$101,279.40 in Fund F8FT, Dept. STS, Unit 88FT, Act. THRG Obj. 4820, Major Program STSMJR, Program TPF8FT16.

Section 4. That the Chief Financial Officer is hereby authorized to disburse funds from Fund F8FT, Dept. STS, Unit 88FT, Act. THRG Obj. 4820, Major Program STSMJR, Program TPF8FT16, in an amount not to exceed \$101,279.40.

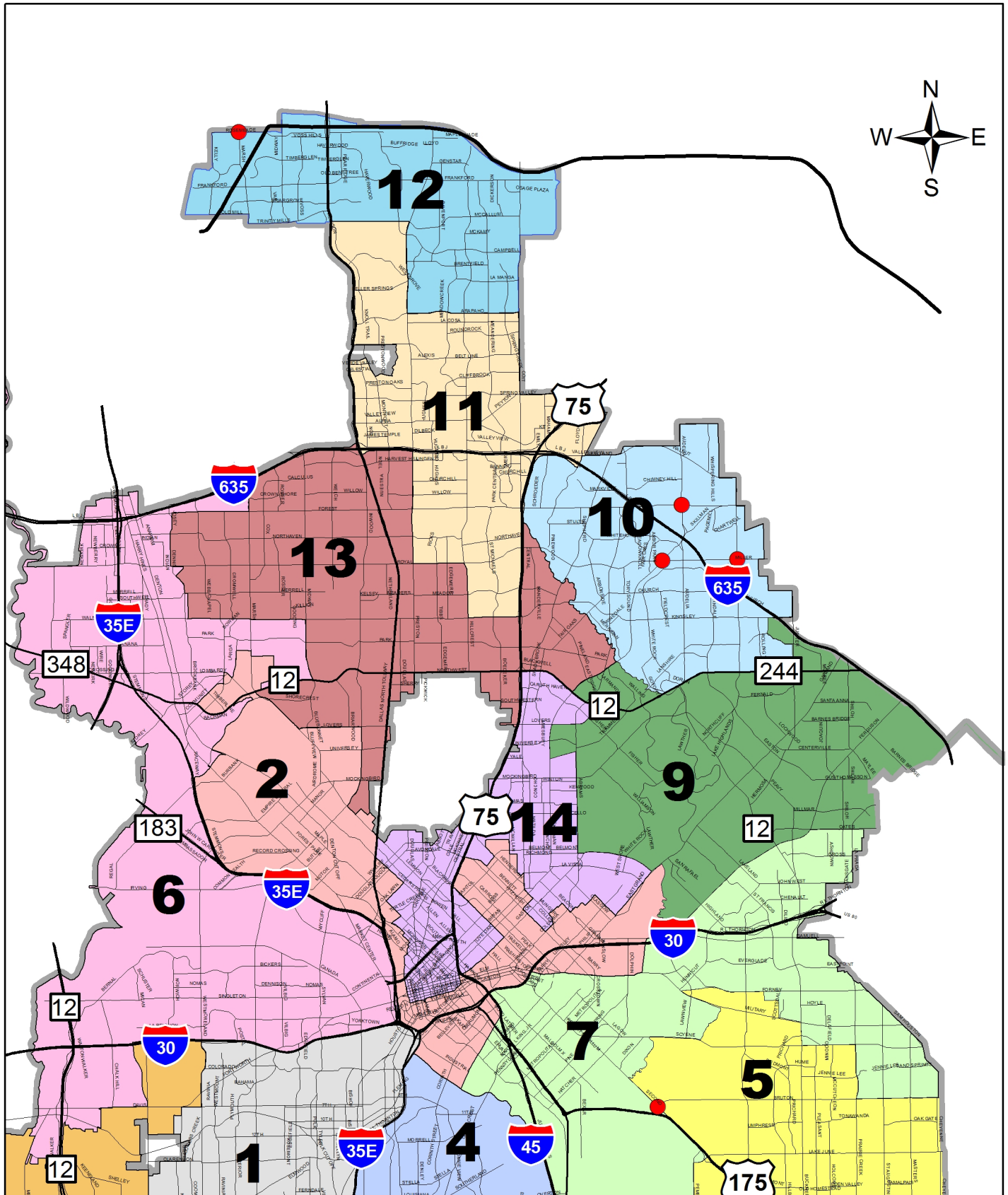
March 22, 2017

Section 5. That the City Manager is hereby authorized to reimburse the granting agency any expenditures identified as ineligible and notify the appropriate City Council Committee of expenditures identified as ineligible not later than 30 days after the reimbursement.

Section 6. That the City Manager shall keep the appropriate City Council Committee informed of all final granting agency monitoring reports not later than 30 days after the receipt of the report.

Section 7. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

2014 HSIP SUBMITTAL FOR 2017 LET 5 SIGNALS



MAPSCO PAGE 17G, 17Y, 28E, 57B, 654Z

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: March 22, 2017
COUNCIL DISTRICT(S): 1, 2
DEPARTMENT: Mobility and Street Services
CMO: Jill A. Jordan, P.E., 670-5299
MAPSCO: 44Z 45P S T W 54 D

SUBJECT

Authorize payment to Dallas Area Rapid Transit for operation and maintenance costs for the Dallas Streetcar System (Union Station to Bishop Arts District) for Fiscal Year 2017 - Not to exceed \$975,000 - Financing: Current Funds

BACKGROUND

The City maintains several Interlocal Agreements (ILA) with the North Central Texas Council of Governments (NCTCOG), Federal Transit Administration (FTA), Texas Department of Transportation (TxDOT), and with Dallas Area Rapid Transit (DART) pertaining to the Dallas Streetcar System. These ILA's established the City of Dallas as the owner of the Dallas Streetcar and allow for the City to rely on technical expertise from DART to operate and maintain the Dallas Streetcar.

On May 28, 2014, City Council authorized the City Manager to negotiate an ILA with DART for the operation and maintenance (O&M) of the Dallas Streetcar Starter System (Union Station to Methodist Hospital) by Resolution No. 14-0831. This ILA, which was executed by the City Manager on September 19, 2014, and executed by DART on January 27, 2015, further defined roles and responsibilities for the ongoing O&M of the streetcar system.

On June 15, 2016, City Council authorized an amendment to the ILA to also include the operation and maintenance of the extensions to the streetcar system, by Resolution No. 16-0980. As expected, the City and DART continue to operate based on the terms outlined in this ILA.

The City annual budget for FY 2016-17 included \$975,000 for Streetcar O&M while DART budgeted \$546,526 for O&M. In accordance with the ILA, DART covers the O&M costs and then invoices the City on a monthly basis. If approved, this action will authorize the payment of funds for O&M up to the allocated amount of \$975,000.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

On May 28, 2014, City Council authorized the City Manager to negotiate an Interlocal Agreement (ILA) with DART to detail agency roles and responsibilities related to operation and maintenance of the TIGER Grant-funded Dallas Streetcar Project (Union Station to North Oak Cliff), by Resolution No. 14-0831.

On June 15, 2016, City Council authorized an amendment to the above ILA to also include the operation and maintenance of the extensions to the streetcar system, by Resolution No. 16-0980.

On June 15, 2016, City Council authorized payment to Dallas Area Rapid Transit for the operation and maintenance of the Dallas Streetcar System for FY 2016 in an amount not to exceed \$875,000.00, by Resolution No. 16-0981.

Information about this item was provided to the Transportation and Trinity River Project Committee on February 27, 2017.

FISCAL INFORMATION

Current Funds - \$975,000.00

<u>Council District</u>	<u>Amount</u>
1	\$877,500.00
2	<u>\$ 97,500.00</u>
Total	\$975,000.00

MAP

Attached

March 22, 2017

WHEREAS, the City of Dallas, Dallas Area Rapid Transit (DART), North Central Texas Council of Governments (NCTCOG) and Texas Department of Transportation (TxDOT) have entered into interlocal agreements (ILA) related to the funding, construction, and operation of the Dallas Streetcar System; and,

WHEREAS, on April 27, 2011, City Council authorized an ILA with NCTCOG and DART to detail agency roles and responsibilities related to the financial commitment for operation and maintenance of the Dallas Streetcar Starter System (Union Station to Methodist Hospital), by Resolution No. 11-1185; and,

WHEREAS, on May 28, 2014, City Council authorized an ILA with DART to detail agency roles and responsibilities related to operation and maintenance (O&M) of the Dallas Streetcar Starter System (Union Station to Methodist Hospital), by Resolution No. 14-0831; and,

WHEREAS, on June 15, 2016, City Council authorized the City Manager to execute an amended said ILA with DART for the operation and maintenance of the Dallas Streetcar System including the southern and northern extensions of the Dallas Streetcar System, two additional vehicles, and any additional Streetcar vehicles ordered in the future, by Resolution No. 16-0980; and,

WHEREAS, the City's adopted annual budget for fiscal year 2016-2017 included up to \$975,000 to reimburse DART for the operation and maintenance of the Dallas Streetcar System.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City hereby authorizes payment to Dallas Area Rapid Transit for the operation and maintenance of the Dallas Streetcar System for FY 2017 in an amount not to exceed \$975,000.00.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in accordance with the terms and conditions of the contract from:

Current Funds

Fund: 0001, Department BMS, Unit 1991

Object: 3070, Program #PBPRP629, CT# PBWPRP629II

Vendor No. 232802, in an amount not to exceed \$ 975,000

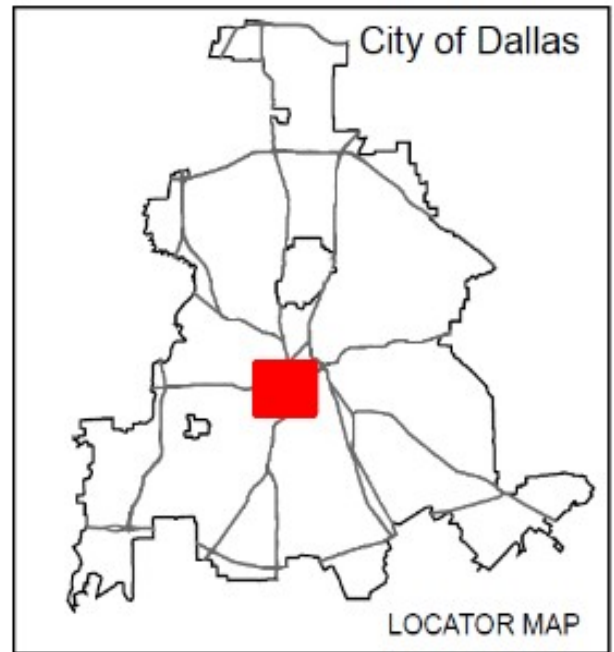
March 22, 2017

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.


Dallas Modern Streetcar Extensions


Districts: 1, 2

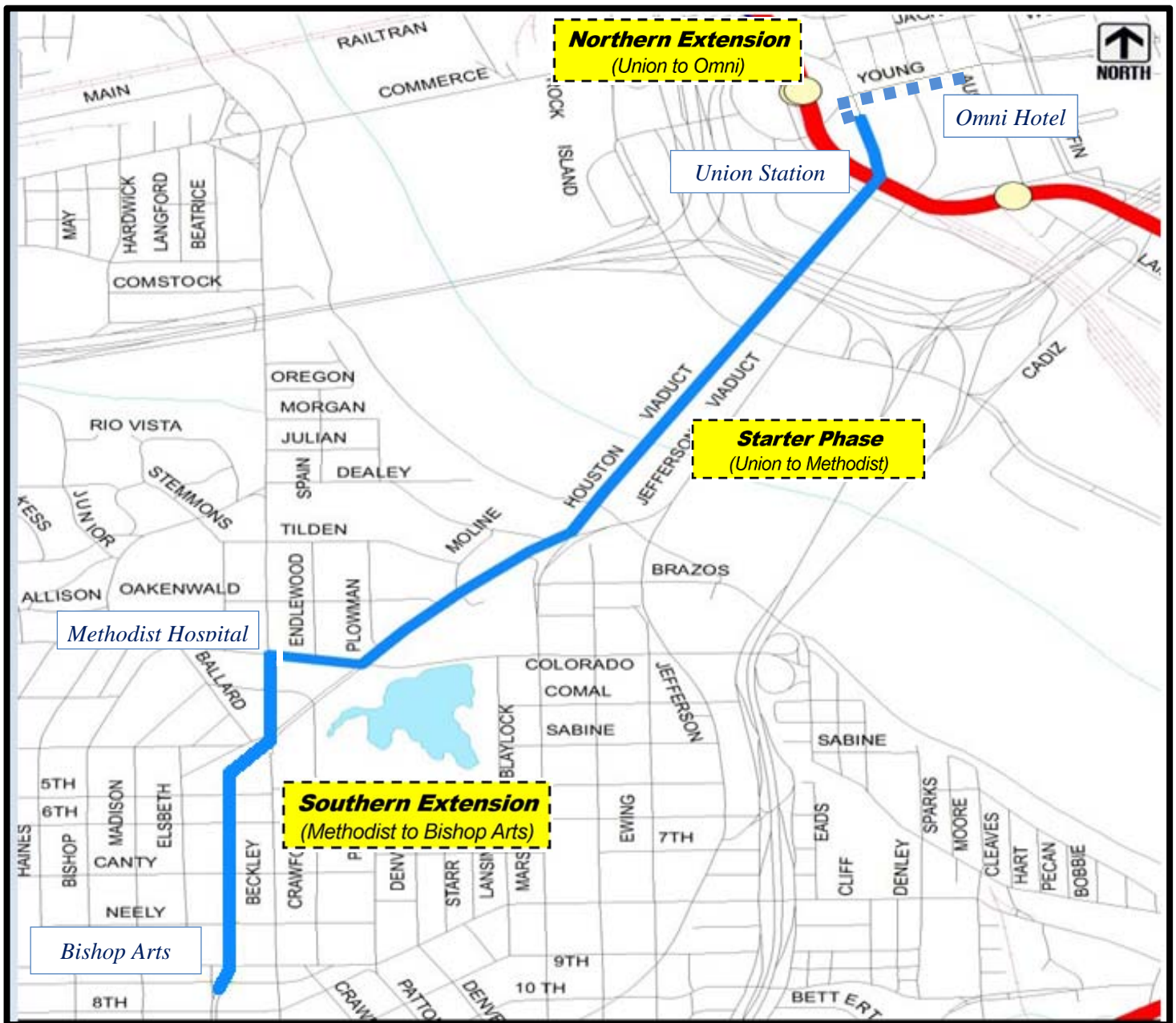
MAPSCO: 44 Z, 45 P, S, T, W, 54 D



LEGEND

STREETCAR ROUTE = 

DART TRAIN ROUTE = 



KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: March 22, 2017

COUNCIL DISTRICT(S): 14

DEPARTMENT: Mobility and Street Services
Water Utilities

CMO: Jill A. Jordan, P.E., 670-5299
Mark McDaniel, 670-3256

MAPSCO: 35Q R V

SUBJECT

Authorize a professional services contract with Huitt-Zollars, Inc. for the engineering design of complete street improvements for Knox Street from Katy Trail to US 75 - Not to exceed \$307,935 - Financing: 2012 Bond Funds (\$288,896) and Water Utilities Capital Construction Funds (\$19,039)

BACKGROUND

Knox Street from Katy Trail to US 75 was funded in the 2012 Bond Program for complete street improvements. On August 24, 2016, a public hearing was conducted and Resolution No. 16-1378 authorized a thoroughfare plan amendment to change the functional and dimensional classifications of Knox Street from Travis Street to McKinney Avenue from a four-lane undivided roadway (M-4-U) within 60-feet of right-of-way to a special three-lane undivided roadway (SPCL 3U) within 60-feet of right-of-way.

This action will authorize a professional services contract with Huitt-Zollars, Inc. for the engineering design of Knox Street from Katy Trail to US 75 complete street improvements determined through a comprehensive 2-year planning process with extensive community involvement. The improvements will consist of modifying the existing roadway to eliminate one lane of traffic and convert the existing four-lane undivided thoroughfare to a two-lane roadway with a center turn lane, and include pull-in parking and sidewalk modifications on the south side of Knox Street. Other improvements will include storm sewer inlet adjustments, water and wastewater replacements, landscaping, irrigation, benches, trash receptacles, bike racks, traffic signal, pedestrian lighting and utility modifications.

ESTIMATED SCHEDULE OF PROJECT

Begin Design	April 2017
Complete Design	December 2017
Begin Construction	April 2018
Complete Construction	March 2019

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

The City Plan Commission Transportation Committee acted on the thoroughfare plan amendment for the conversion of Knox Street between Travis Street and McKinney Avenue from a four-lane undivided roadway to a three-lane undivided roadway on January 21, 2016, and followed staff recommendation of approval.

The City Plan Commission acted on this item on February 4, 2016, held under advisement; March 4, 2016, held under advisement; May 19, 2016, held under advisement; and July 21, 2016, and followed staff recommendation of approval.

A public hearing was conducted to authorize a thoroughfare plan amendment to change the functional and dimensional classifications of Knox Street between Travis Street and McKinney Avenue from a four-lane undivided roadway (M-4-U) within 60-feet of right-of-way to a special three-lane undivided roadway (SPCL 3U) within 60-feet of right-of-way on August 24, 2016, by Resolution No. 16-1378.

Information about this item will be provided to the Transportation and Trinity River Project Committee on February 27, 2017.

FISCAL INFORMATION

2012 Bond Funds - \$288,895.50

Water Utilities Capital Construction Funds - \$19,039.00

Design (this action)	\$ 307,934.50
Construction	
Paving & Drainage - STS	\$1,869,354.80 (est.)
Water & Wastewater - DWU	<u>\$ 157,354.70 (est.)</u>
Total	\$2,334,644.00 (est.)

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

Huitt-Zollars, Inc.

Hispanic Female	9	Hispanic Male	12
African-American Female	7	African-American Male	2
Other Female	2	Other Male	6
White Female	29	White Male	49

OWNER

Huitt-Zollars, Inc.

John Ho, P.E., Vice President

MAP

Attached.

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize a professional services contract with Huitt-Zollars, Inc. for the engineering design of complete street improvements for Knox Street from Katy Trail to US 75 - Not to exceed \$307,935 - Financing: 2012 Bond Funds (\$288,896) and Water Utilities Capital Construction Funds (\$19,039)

Huitt-Zollars, Inc. is a local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Architecture & Engineering

LOCAL/NON-LOCAL CONTRACT SUMMARY

	<u>Amount</u>	<u>Percent</u>
Total local contracts	\$307,934.50	100.00%
Total non-local contracts	\$0.00	0.00%
TOTAL CONTRACT	\$307,934.50	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION

Local Contractors / Sub-Contractors

<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Urban Engineers Group, Inc.	HFDB47619Y0817	\$48,200.00	15.65%
HVJ North Texas	IMDB60714N0617	\$6,000.00	1.95%
Lim & Associates, Inc.	PMDB90825Y0617	\$48,005.00	15.59%
Total Minority - Local		\$102,205.00	33.19%

Non-Local Contractors / Sub-Contractors

None

TOTAL M/WBE CONTRACT PARTICIPATION

	<u>Local</u>	<u>Percent</u>	<u>Local & Non-Local</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$48,200.00	15.65%	\$48,200.00	15.65%
Asian American	\$54,005.00	17.54%	\$54,005.00	17.54%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$0.00	0.00%	\$0.00	0.00%
Total	\$102,205.00	33.19%	\$102,205.00	33.19%

March 22, 2017

WHEREAS, the City Plan Commission Transportation Committee acted on the Thoroughfare Plan Amendment for the conversion of Knox Street between Travis Street and McKinney Avenue from a four-lane undivided roadway to a three-lane undivided roadway on January 21, 2016, and followed staff recommendation of approval.

WHEREAS, the City Plan Commission acted on this item on February 4, 2016, held under advisement; March 4, 2016, held under advisement; May 19, 2016, held under advisement; and July 21, 2016, and followed staff recommendation of approval, and;

WHEREAS, on August 24, 2016, Resolution No. 16-1378 authorized a thoroughfare plan amendment to change the functional and dimensional classifications of Knox Street between Travis Street and McKinney Avenue from a four-lane undivided roadway (M-4-U) within 60-feet of right-of-way to a special three-lane undivided roadway (SPCL 3U) within 60-feet of right-of-way, and;

WHEREAS, Huitt-Zollars, Inc. was selected to provide the engineering design of complete street improvements for Knox Street from Katy Trail to US 75.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to execute a professional services contract with Huitt-Zollars, Inc. for the engineering design of complete street improvements for Knox Street from Katy Trail to US 75, in an amount not to exceed \$307,934.50, after it has been approved as to form by the City Attorney.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in accordance with the terms and conditions of the contract from:

Street and Transportation Improvements Fund Fund 3U22, Department STS, Unit S408, Act. CPST Obj. 4111, Program #PB12S408, CT STS12S408F1 Vendor #090025, in an amount not to exceed	\$288,895.50
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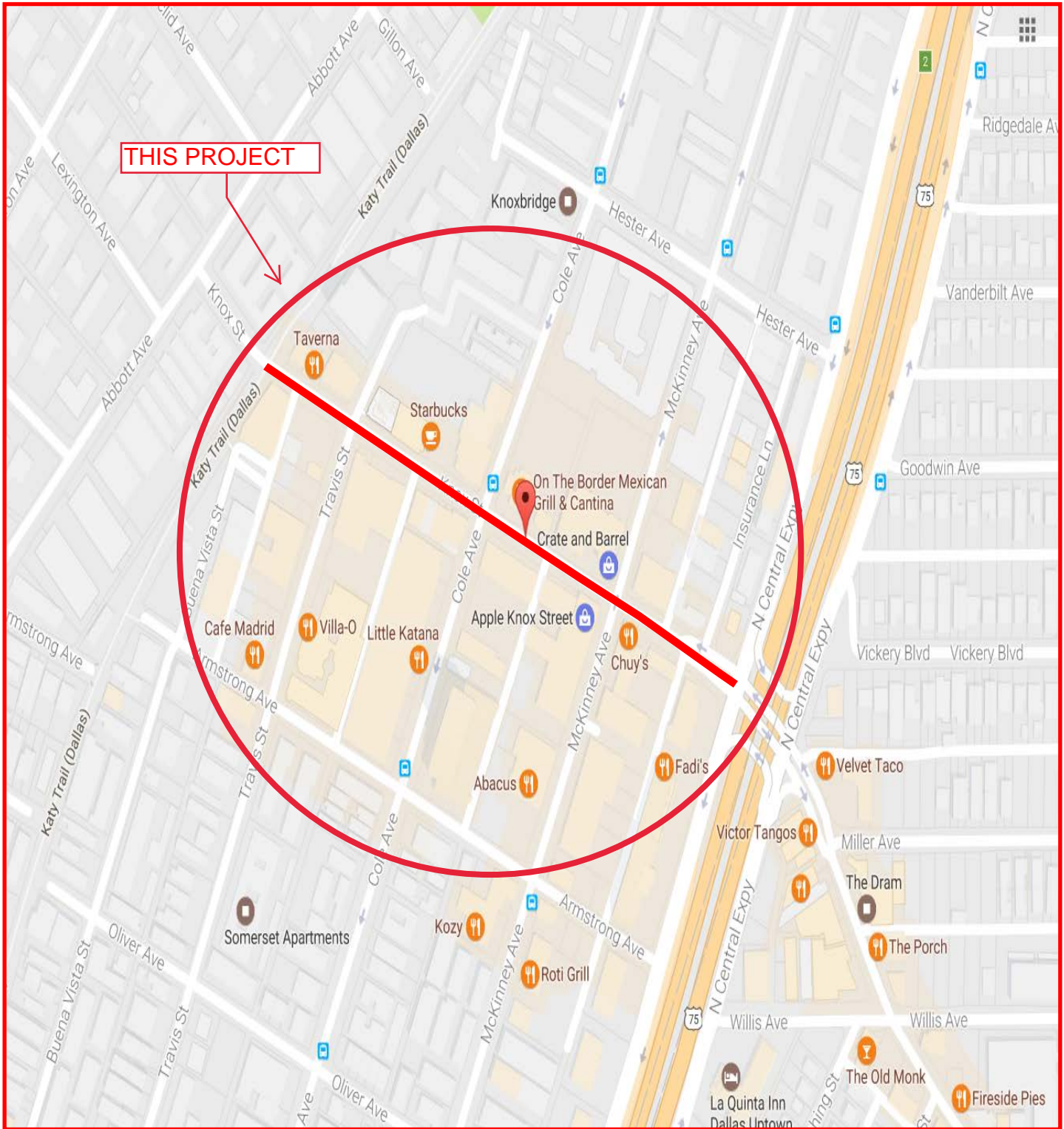
Water Construction Fund Fund 0102, Department DWU, Unit CW42 Obj. 4111, Program #717179, CT PBW717179EN Vendor #090025, in an amount not to exceed	\$ 11,423.40
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March 22, 2017

Wastewater Construction Fund	
Fund 0103, Department DWU, Unit CS42	
Obj. 4111, Program #717180, CT PBW717180EN	
Vendor #090025, in an amount not to exceed	<u>\$ 7,615.60</u>
 Total amount not to exceed	 \$307,934.50

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KNOX STREET FROM KATY TRAIL TO US 75



MAPSCO 35Q, 35R, 35V

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: March 22, 2017

COUNCIL DISTRICT(S): 3, 11, 13

DEPARTMENT: Mobility and Street Services
Water Utilities

CMO: Jill A. Jordan, P.E., 670-5299
Mark McDaniel, 670-3256

MAPSCO: 24U 25F G H 26E W 61B-J

SUBJECT

Authorize a construction contract with Ark Contracting Services, LLC, lowest responsible bidder of four, for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2040 and 12-2045, and paving improvements for Merrifield Road Dead End Turnaround Project (list attached) - Not to exceed \$4,174,616 - Financing: 2012 Bond Funds (\$1,658,794), 2006 Bond Funds (\$163,430), General Obligation Commercial Paper Funds (\$234,200), Water Utilities Capital Improvement Funds (\$2,094,667) and Water Utilities Capital Construction Funds (\$23,525)

BACKGROUND

Alley Reconstruction Groups 12-2040 and 12-2045 were approved in the 2012 Bond Program. On September 25, 2013, Resolution No. 13-1731 authorized professional services contracts for the engineering design services with ARS Engineers, Inc. for Alley Reconstruction Group 12-2040 and Multatech Engineering, Inc. for Alley Reconstruction Group 12-2045. Engineering design was provided by The Courtland Group for the Merrifield Road Dead End Turnaround Project. This action will authorize a construction contract for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Group 12-2040, which consists of the alley between Azalea Lane and Royalton Drive from Azalea Lane to Tibbs Street; the alley between Azalea Lane and Currin Drive from Saint Judes Drive to Michaels Drive; the alley between Lavendale Avenue and Royalton Drive from Tibbs Street to Lavendale Avenue; the alley between Lavendale Avenue and Azalea Lane from Camelia Drive to Tibbs Street; the alley between Lavendale Avenue and Royal Lane from Hillcrest Road to Saint Judes Drive; the alley between Lavendale Avenue and Royal Lane from Tibbs Street to Edgemere Road; and the alley between Lavendale Avenue and Royal Lane from Royal Lane to Tibbs Street, and Alley Reconstruction Group 12-4045, which consists of the alley

BACKGROUND (Continued)

between Brookview Drive and Gloster Road from Manchester Drive to Rockbrook Drive; and the alley between Caruth Boulevard and Colgate Avenue from Durham Street to Boedeker Street.

Alley Reconstruction Groups 12-2040 and 12-2045 include the construction of reinforced concrete alley pavement, storm drainage, drive approaches and wastewater main and adjustment improvements. The Merrifield Road Dead End Turnaround Project includes the construction of an 11-inch thick reinforced concrete pavement and other miscellaneous items necessary to complete the project.

The construction of Merrifield Road ended at approximately 3825 feet north of Mountain Creek Parkway to allow for extending Merrifield Road to Grady Niblo Road. However, trucks turning at the dead end of Merrifield Road have damaged the median and a section of the existing pavement. It is now necessary to construct a turnaround at the Merrifield Road dead end to provide easy turning for trucks and other motorists.

Ark Contracting Services, LLC has no completed contractual activities with the City of Dallas within the past three years.

ESTIMATED SCHEDULE OF PROJECT

Alley Reconstruction Group 12-2040

Began Design	November 2013
Completed Design	November 2016
Begin Construction	May 2017
Complete Construction	August 2018

Alley Reconstruction Group 12-2045

Began Design	November 2013
Completed Design	November 2016
Begin Construction	May 2017
Complete Construction	August 2018

Merrifield Road Dead End Turnaround

Began Design	Designed by the Developer
Completed Design	November 2016
Begin Construction	May 2017
Complete Construction	August 2018

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized an infrastructure cost participation agreement between the City of Dallas and The Courtland Group on June 28, 2000, by Resolution No. 00-2141.

Authorized a construction contract with Mario Sinacola and Sons Excavating, Inc. for the Merrifield Road Extension on October 10, 2012, by Resolution No. 12-2547.

Authorized a professional services contract with ARS Engineers, Inc. for engineering design services on September 25, 2013, by Resolution No. 13-1731.

Authorized a professional services contract with Multatech Engineering, Inc. for engineering design services on September 25, 2013, by Resolution No. 13-1731.

Information about this item will be provided to the Transportation and Trinity River Project Committee on February 27, 2017.

FISCAL INFORMATION

2006 Bond Funds - \$163,430
2012 Bond Funds - \$1,658,794
2012 Bond Program (General Obligation Commercial Paper Funds) - \$234,200
Water Utilities Capital Improvement Funds - \$2,094,667
Water Utilities Capital Construction Funds - \$23,525

Alley Reconstruction Group 12-2040

Design - STS	\$ 274,101.89
Design - DWU	\$ 60,275.00

Alley Reconstruction Group 12-2045

Design - STS	\$ 63,953.00
Design - DWU	\$ 18,332.00

Merrifield Road Dead End Turnaround

Design - (by the developer)	\$ 0.00
Construction (this action)	
Paving & Drainage - STS	\$2,056,424.00
Wastewater - DWU	<u>\$2,118,192.00</u>
Total Project Cost	\$4,591,277.89

FISCAL INFORMATION (Continued)

<u>Council District</u>	<u>Amount</u>
3	\$ 902,509.00
11	\$3,108,677.00
13	<u>\$ 163,430.00</u>
Total	\$4,174,616.00

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

Ark Contracting Services, LLC

Hispanic Female	0	Hispanic Male	131
African-American Female	0	African-American Male	0
Other Female	0	Other Male	0
White Female	5	White Male	21

BID INFORMATION

The following bids with quotes were received and opened on December 23, 2016:

*Denotes successful bidder(s)

<u>BIDDERS</u>	<u>BID AMOUNT</u>
*Ark Contracting Services, LLC 420 S. Dick Price Road Kennedale, Texas 76060	\$4,174,616.00
Vescorp Construction	\$4,724,055.72
Camino Construction, LP	\$4,994,856.00
Jeske Construction Company	\$5,468,823.00

Original estimate: STS - \$1,955,095.85
WTR - \$1,357,999.00

Total \$3,313,094.85

OWNER

Ark Contracting Services, LLC

Steven C. Bowman, President

MAPS

Attached.

**Alley Reconstruction Groups 12-2040 and 12-2045
and Paving Improvements**

<u>Paving Improvements</u>	<u>Council District</u>
Merrifield Road Dead End Turnaround	3
<u>Alley Reconstruction Group 12-2040</u>	
Alley between Azalea Lane and Royalton Drive from Azalea Lane to Tibbs Street	11
Alley between Azalea Lane and Currin Drive from Saint Judes Drive to Saint Michaels Drive	11
Alley between Lavendale Avenue and Royalton Drive from Tibbs Street to Lavendale Avenue	11
Alley between Lavendale Avenue and Azalea Lane from Camelia Drive to Tibbs Street	11
Alley between Lavendale Avenue and Royal Lane from Hillcrest Road to Saint Judes Drive	11
Alley between Lavendale Avenue and Royal Lane from Tibbs Street to Edgemere Road	11
Alley between Lavendale Avenue and Royal Lane from Royal Lane to Tibbs Street	11
<u>Alley Reconstruction Group 12-2045</u>	
Alley between Brookview Drive and Gloster Road from Manchester Drive to Rockbrook Drive	13
Alley between Caruth Boulevard and Colgate Avenue from Durham Street to Boedeker Street	13

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize a construction contract with Ark Contracting Services, LLC, lowest responsible bidder of four, for the reconstruction of alley paving, storm drainage, drive approaches, and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2040 and 12-2045, and paving improvements for Merrifield Road Dead End Turnaround Project (list attached) - Not to exceed \$4,174,616 - Financing: 2012 Bond Funds (\$1,658,794), 2006 Bond Funds (\$163,430), General Obligation Commercial Paper Funds (\$234,200), Water Utilities Capital Improvement Funds (\$2,094,667) and Water Utilities Capital Construction Funds (\$23,525)

Ark Contracting, Inc. is a non-local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Construction

LOCAL/NON-LOCAL CONTRACT SUMMARY

	<u>Amount</u>	<u>Percent</u>
Total local contracts	\$1,535,000.00	36.77%
Total non-local contracts	\$2,639,616.00	63.23%
TOTAL CONTRACT	\$4,174,616.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION

Local Contractors / Sub-Contractors

<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
J & A Trucking	HMMB64410N0317	\$260,000.00	16.94%
Reyes Group, Ltd.	HMMB45079Y0818	\$20,000.00	1.30%
J. E. Guzman Construction	HMMB64868N0417	\$10,000.00	0.65%
LKT & Associates	WFDB26473Y1016	\$300,000.00	19.54%
Magnum Manhole & Underground	WFDB06880Y0617	\$50,000.00	3.26%
Total Minority - Local		\$640,000.00	41.69%

Non-Local Contractors / Sub-Contractors

<u>Non-local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Viking Fence Company	HMMB31501N0617	\$20,000.00	0.76%
Cowtown Redi Mix	WFWB06682Y0916	\$360,000.00	13.64%
ANA Consultants, LLC	WFDB96312Y0617	\$30,000.00	1.14%
Brock Environmental	WFWB96114Y1118	\$10,000.00	0.38%
Total Minority - Non-local		\$420,000.00	15.91%

TOTAL M/WBE CONTRACT PARTICIPATION

	<u>Local</u>	<u>Percent</u>	<u>Local & Non-Local</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$290,000.00	18.89%	\$310,000.00	7.43%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$350,000.00	22.80%	\$750,000.00	17.97%
Total	<u>\$640,000.00</u>	<u>41.69%</u>	<u>\$1,060,000.00</u>	<u>25.39%</u>

March 22, 2017

WHEREAS, on June 28, 2000, Resolution No. 00-2141 authorized an infrastructure cost participation agreement between the City of Dallas and The Courtland Group in the amount of \$7,000,000.00; and,

WHEREAS, on October 10, 2012, Resolution No. 12-2547 authorized a construction contract with Mario Sinacola and Sons Excavating, Inc. for the construction of the Merrifield Road Extension Project in the amount of \$1,519,172.69; and,

WHEREAS, on September 25, 2013, Resolution No. 13-1731 authorized a professional services contract with ARS Engineers, Inc. for the engineering design services for Street Reconstruction Group 12-2040, in the amount of \$307,376.89; and,

WHEREAS, on September 25, 2013, Resolution No. 13-1731 authorized a professional services contract with Multatech Engineering, Inc. for the engineering design services for Alley Reconstruction Group 12-2045, in the amount of \$82,285.00; and,

WHEREAS, on November 11, 2014, Administrative Action No. 14-7193 authorized Supplemental Agreement No. 1 to the professional services contract with ARS Engineers, Inc. to provide additional engineering design services for Alley Reconstruction Group 12-2040 in the amount of \$4,530.00, from \$307,376.89 to \$311,906.89; and,

WHEREAS, on December 9, 2015, Administrative Action No. 15-7452 authorized Supplemental Agreement No. 2 to the professional services contract with ARS Engineers, Inc. to provide additional engineering design services for Alley Reconstruction Group 12-2040 in the amount of \$31,370.00, from \$311,906.89 to \$343,276.89; and,

WHEREAS, bids were received on December 23, 2016, for the reconstruction of alley paving, storm drainage, drive approaches and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2040 and 12-2045, and paving improvements for Merrifield Road Dead End Turnaround Project, as follows:

<u>BIDDERS</u>	<u>BID AMOUNT</u>
Ark Contracting Services, LLC	\$4,174,616.00
Vescorp Construction	\$4,724,055.72
Camino Construction, LP	\$4,994,856.00
Jeske Construction Company	\$5,468,823.00

March 22, 2017**Now, Therefore,****BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:**

Section 1. That the City Manager is hereby authorized to enter into a construction contract with Ark Contracting Services, LLC for the reconstruction of alley paving, storm drainage, drive approaches and wastewater main and adjustment improvements for Alley Reconstruction Groups 12-2040 and 12-2045, and paving improvements for Merrifield Road Dead End Turnaround Project in the amount of exceed \$4,174,616.00, this being the lowest responsible bid received as indicated by the tabulation of bids, after it has been approved as to form by the City Attorney.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in accordance with the terms and conditions of the contract from:

Street and Transportation Improvements Fund Fund 3U22, Department STS, Unit S217, Act. AREC Obj. 4510, Program #PB12S217, CT STS12S217F1 Vendor #VS0000017816, in amount not to exceed	\$ 153,498.00
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Street and Transportation Improvements Fund Fund 3U22, Department STS, Unit S218, Act. AREC Obj. 4510, Program #PB12S218, CT STS12S217F1 Vendor #VS0000017816, in amount not to exceed	\$ 191,377.00
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Street and Transportation Improvements Fund Fund 3U22, Department STS, Unit S277, Act. AREC Obj. 4510, Program #PB12S277, CT STS12S217F1 Vendor #VS0000017816, in amount not to exceed	\$ 275,408.00
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Street and Transportation Improvements Fund Fund 3U22, Department STS, Unit S278, Act. AREC Obj. 4510, Program #PB12S278, CT STS12S217F1 Vendor #VS0000017816, in amount not to exceed	\$ 263,853.00
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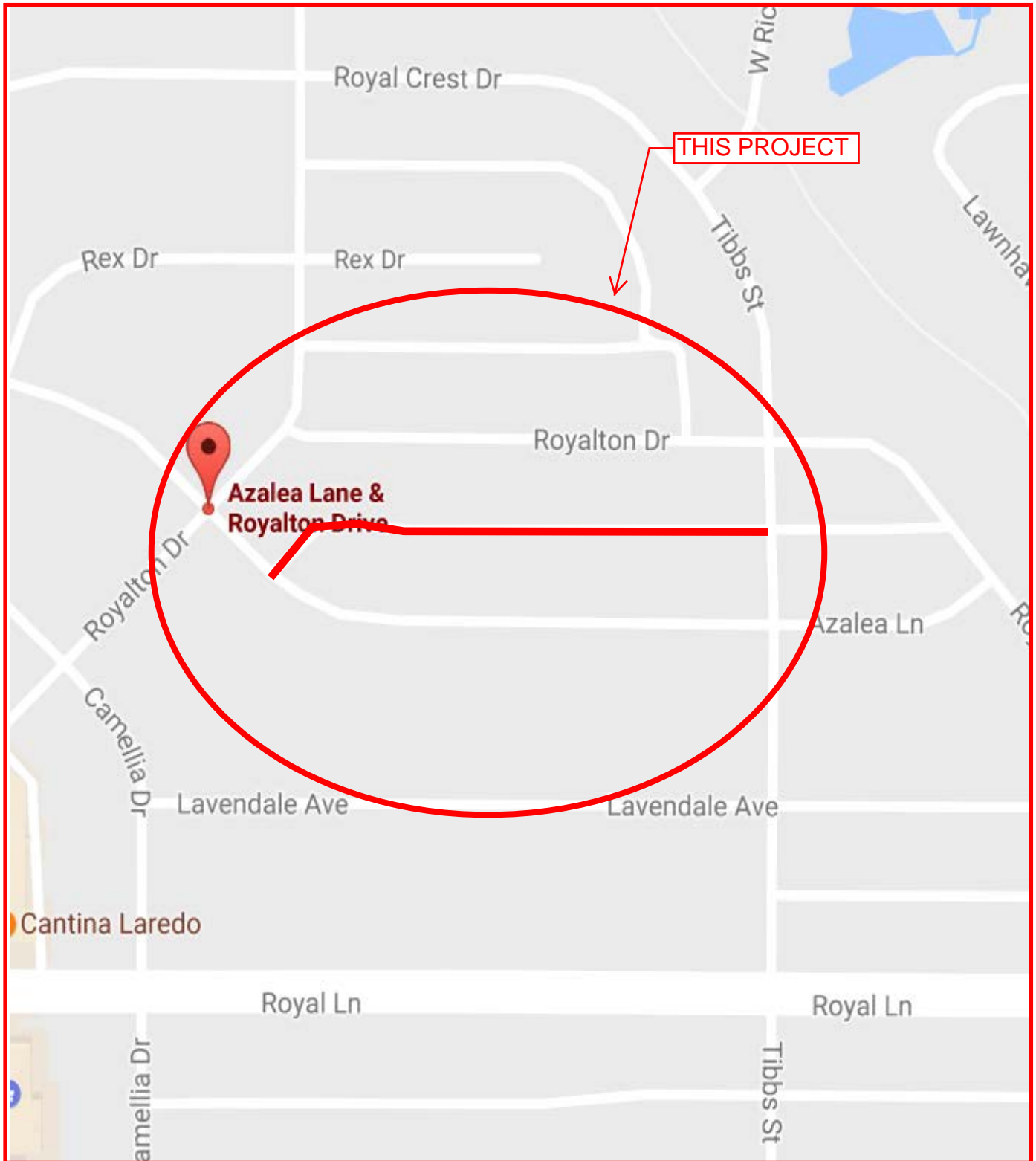
Street and Transportation Improvements Fund Fund 3U22, Department STS, Unit S279, Act. AREC Obj. 4510, Program #PB12S279, CT STS12S217F1 Vendor #VS0000017816, in amount not to exceed	\$ 174,758.00
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March 22, 2017**Section 2.** (continued)

Street and Transportation Improvements Fund Fund 3U22, Department STS, Unit S280, Act. AREC Obj. 4510, Program #PB12S280, CT STS12S217F1 Vendor #VS0000017816, in amount not to exceed	\$ 139,956.00
Street and Transportation Improvements Fund Fund 4U22, Department STS, Unit S281, Act. AREC Obj. 4510, Program #PB12S281, CT STS12S217F1 Vendor #VS0000017816, in amount not to exceed	\$ 234,200.00
Street and Transportation Improvements Fund Fund 3U22, Department STS, Unit S223, Act. AREC Obj. 4510, Program #PB12S223, CT STS12S217F1 Vendor #VS0000017816, in amount not to exceed	\$ 247,320.00
Street and Transportation Improvements Fund Fund 3U22, Department STS, Unit S226, Act. AREC Obj. 4510, Program #PB12S226, CT STS12S217F1 Vendor #VS0000017816, in amount not to exceed	\$ 212,624.00
Street and Transportation Improvements Fund Fund 1T22, Department STS, Unit U794, Act. THRF Obj. 4510, Program #PB06U794, CT STS12S217F1 Vendor #VS0000017816, in amount not to exceed	\$ 163,430.00
Wastewater Capital Improvement Fund Fund 3116, Department DWU, Unit PS42 Obj. 4560, Program #716024, CT PBW716024CP Vendor #VS0000017816, in amount not to exceed	\$2,094,667.00
Wastewater Construction Fund Fund 0103, Department DWU, Unit CS42 Obj. 3222, Program #716024X, CT PBW716024EN Vendor #VS0000017816, in amount not to exceed	<u>\$ 23,525.00</u>
Total amount not to exceed	\$4,174,616.00

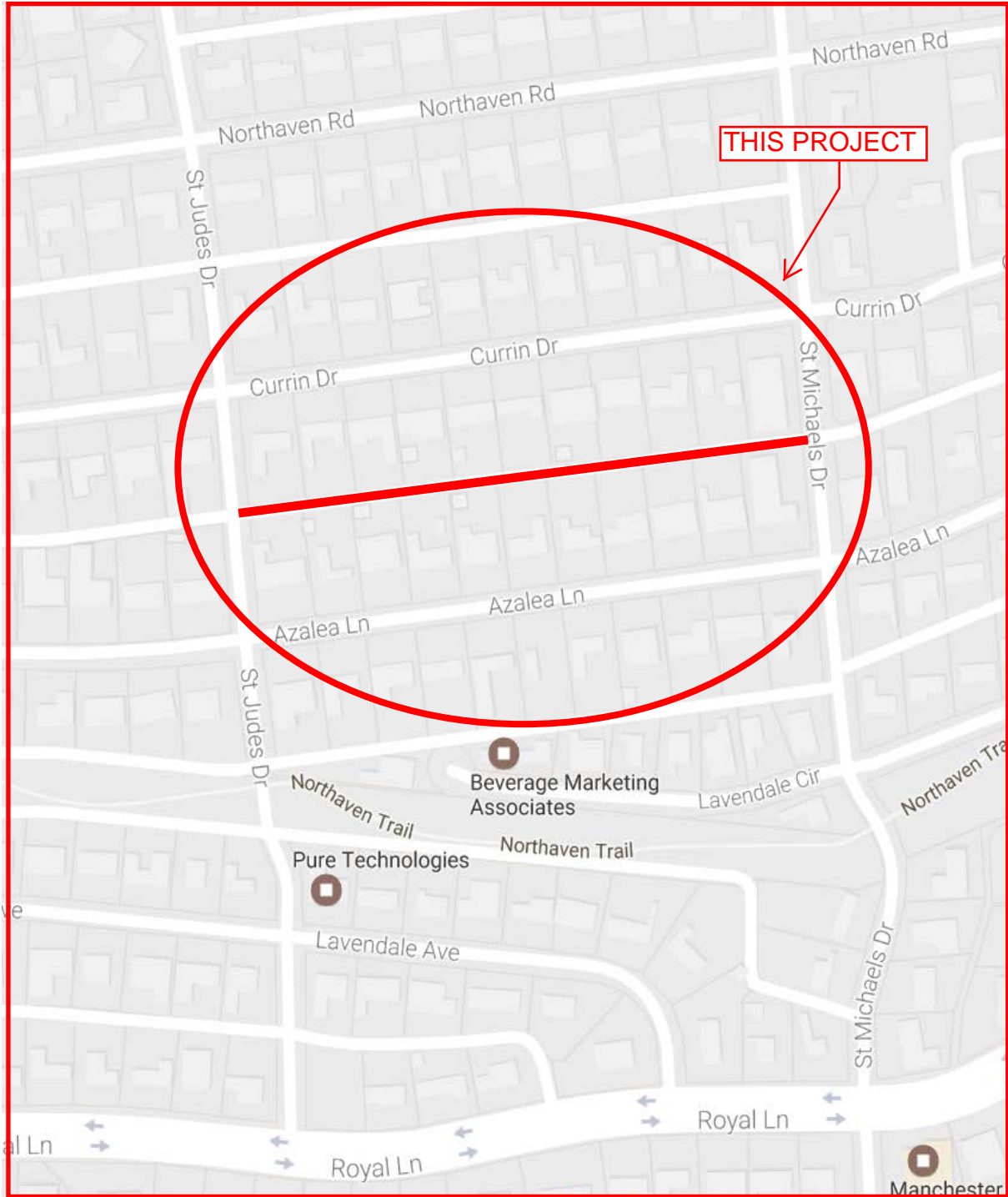
Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

**ALLEY RECONSTRUCTION GROUP 12-2040
ALLEY BETWEEN AZALEA LANE AND ROYALTON DRIVE
FROM AZALEA LANE TO TIBBS STREET**



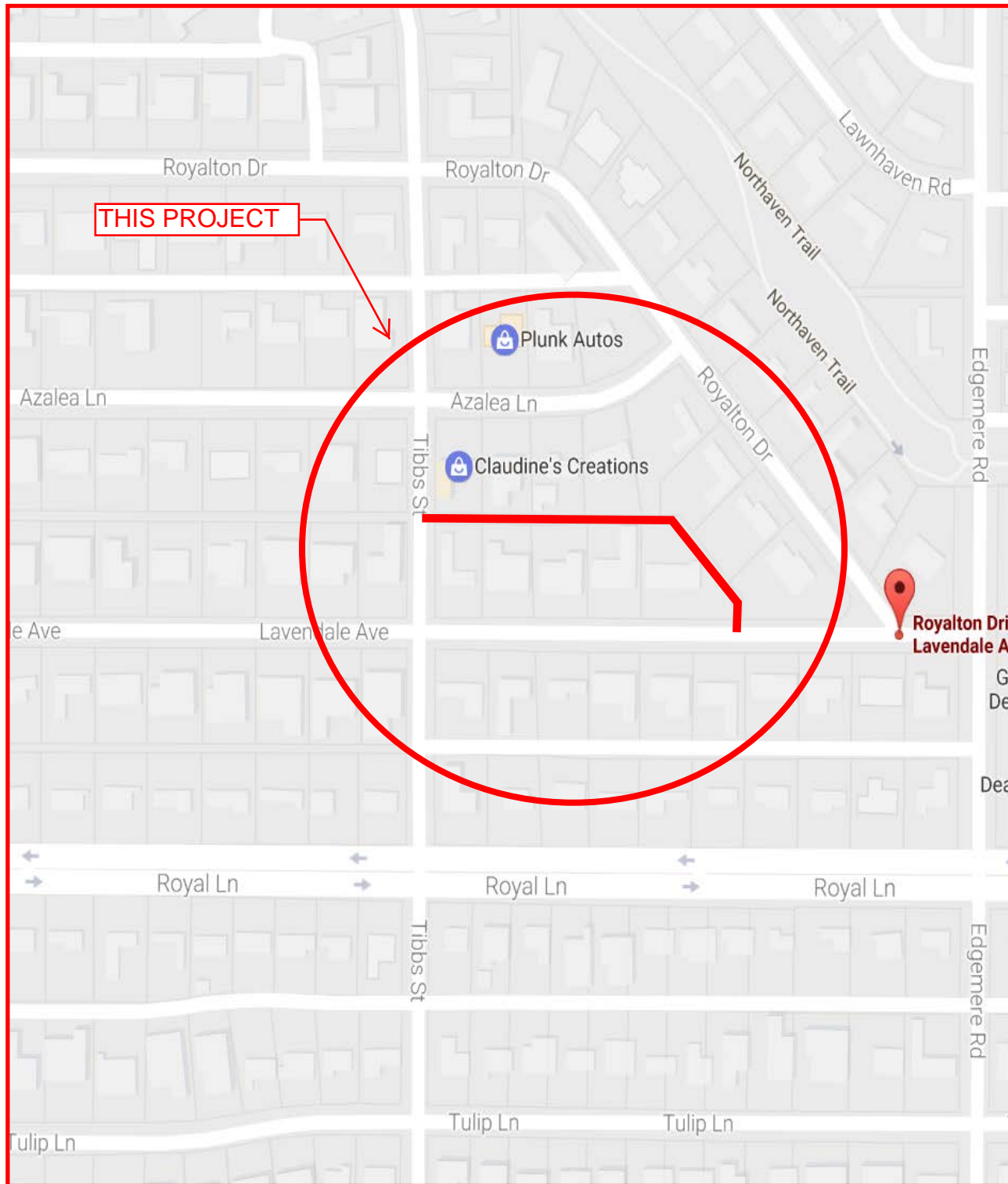
MAPSCO 25F, 25G

**ALLEY RECONSTRUCTION GROUP 12-2040
ALLEY BETWEEN AZALEA LANE AND CURRIN DRIVE
FROM ST. JUDES DRIVE TO ST. MICHAELS DRIVE**



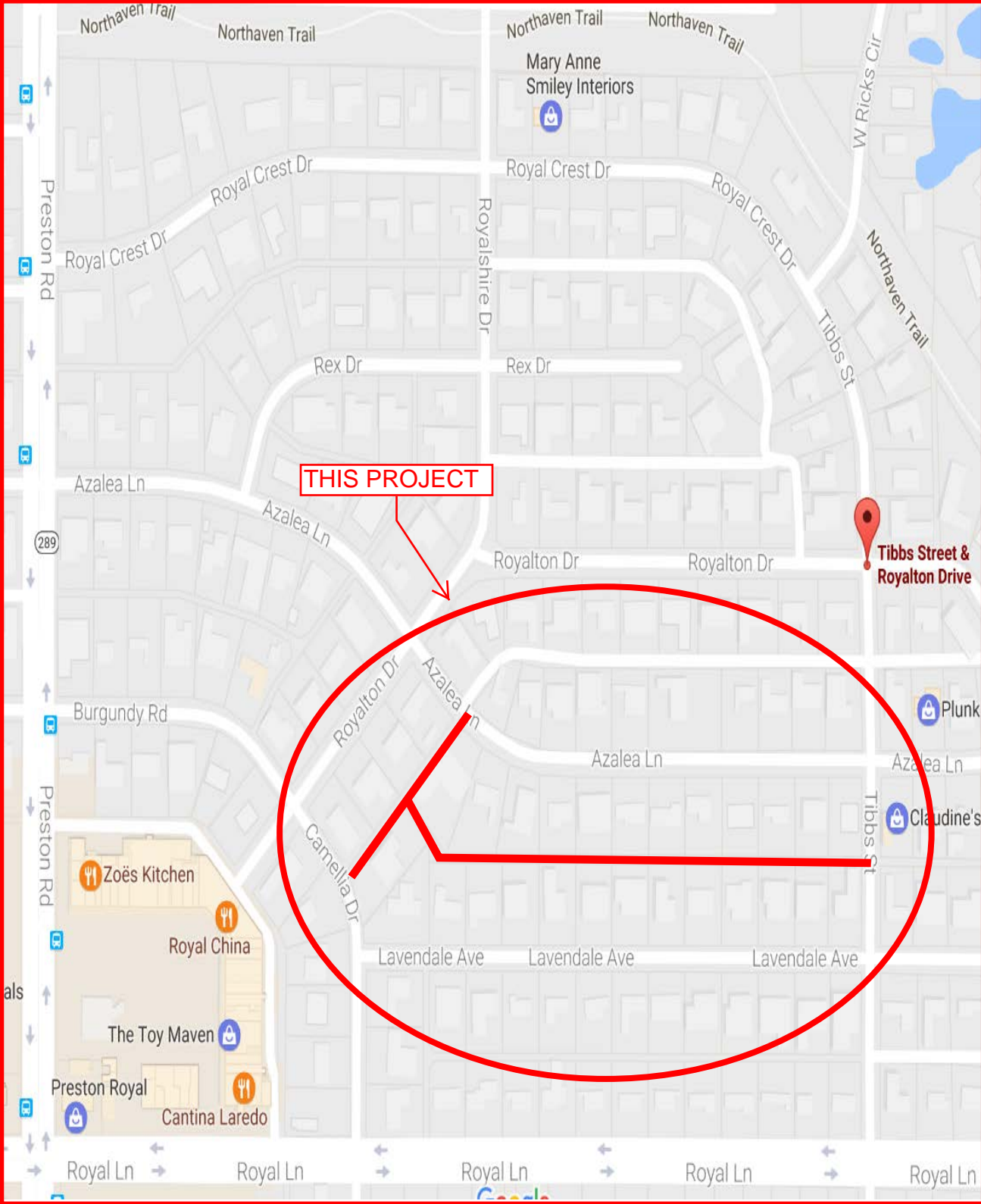
MAPSCO 25H 26E

**ALLEY RECONSTRUCTION GROUP 12-2040
ALLEY BETWEEN LAVENDALE AVENUE AND ROYALTON DRIVE
FROM TIBBS STREET TO LAVENDALE AVENUE**



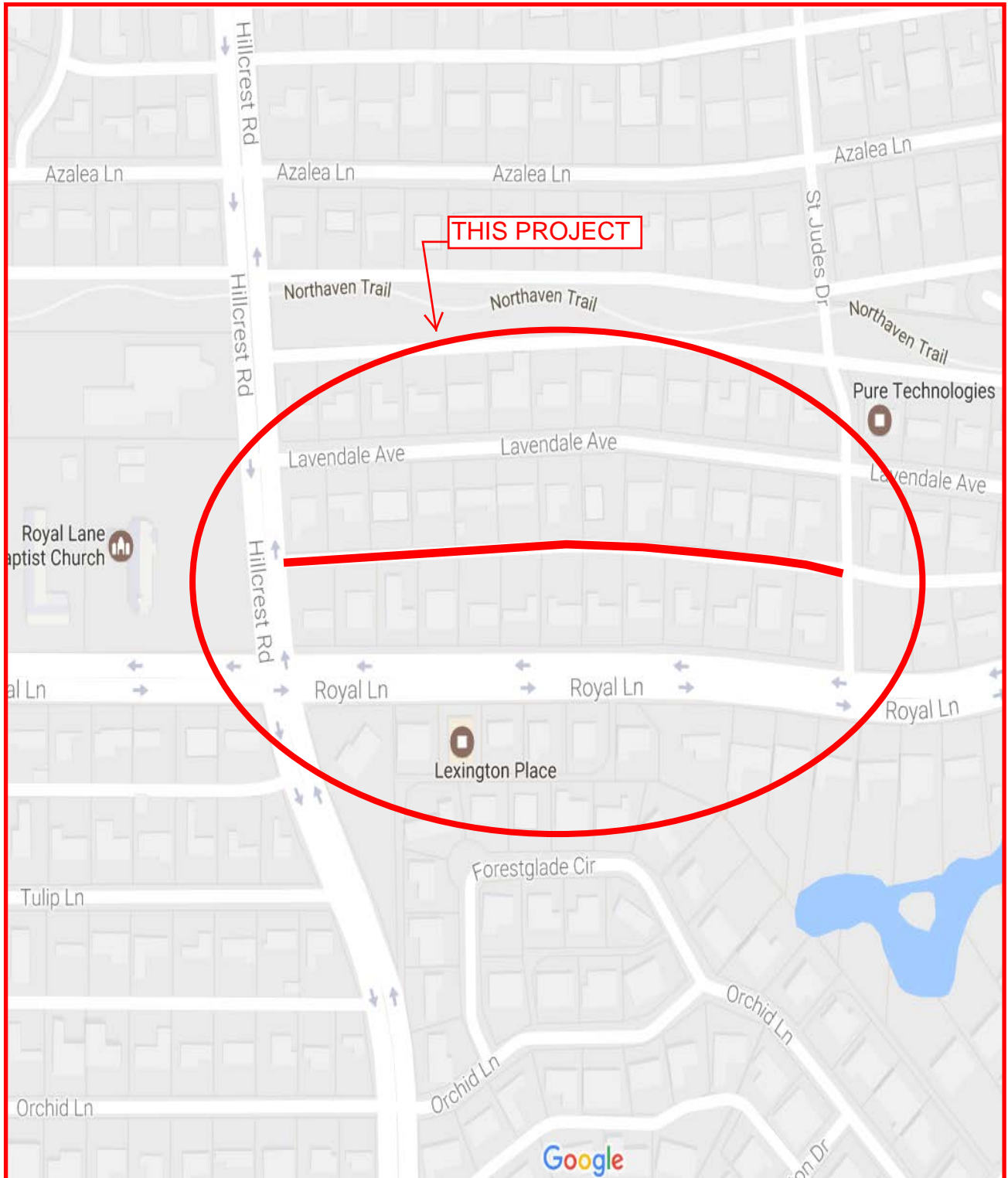
MAPSCO 25G

**ALLEY RECONSTRUCTION GROUP 12-2040
ALLEY BETWEEN LAVENDALE AVENUE AND AZALEA LANE
FROM CAMELIA DRIVE TO TIBBS DRIVE**



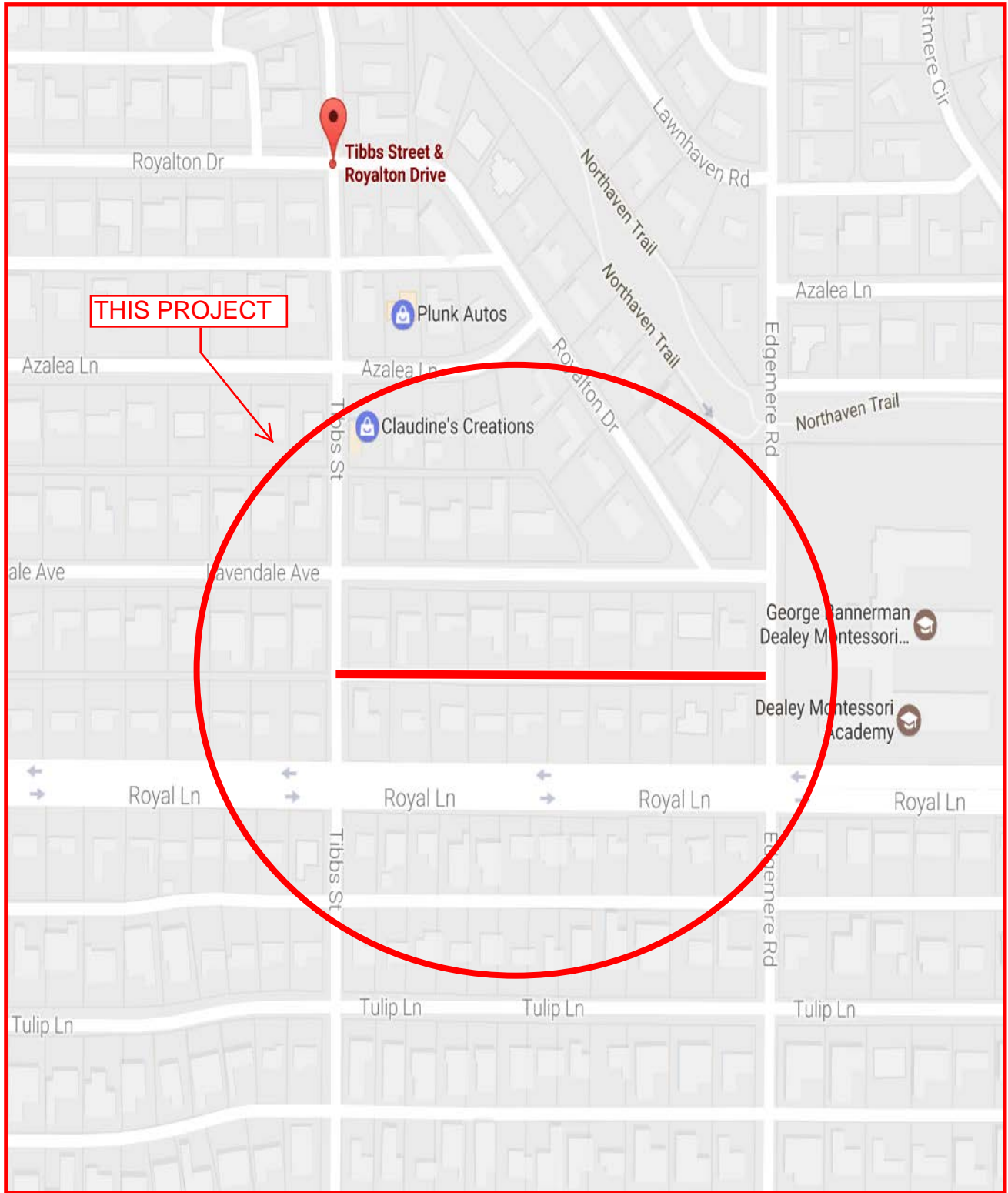
MAPSCO 25F 25G

**ALLEY RECONSTRUCTION GROUP 12-2040
ALLEY BETWEEN LAVENDALE AVENUE AND ROYAL LANE
FROM HILLCREST ROAD TO ST. JUDES DRIVE**



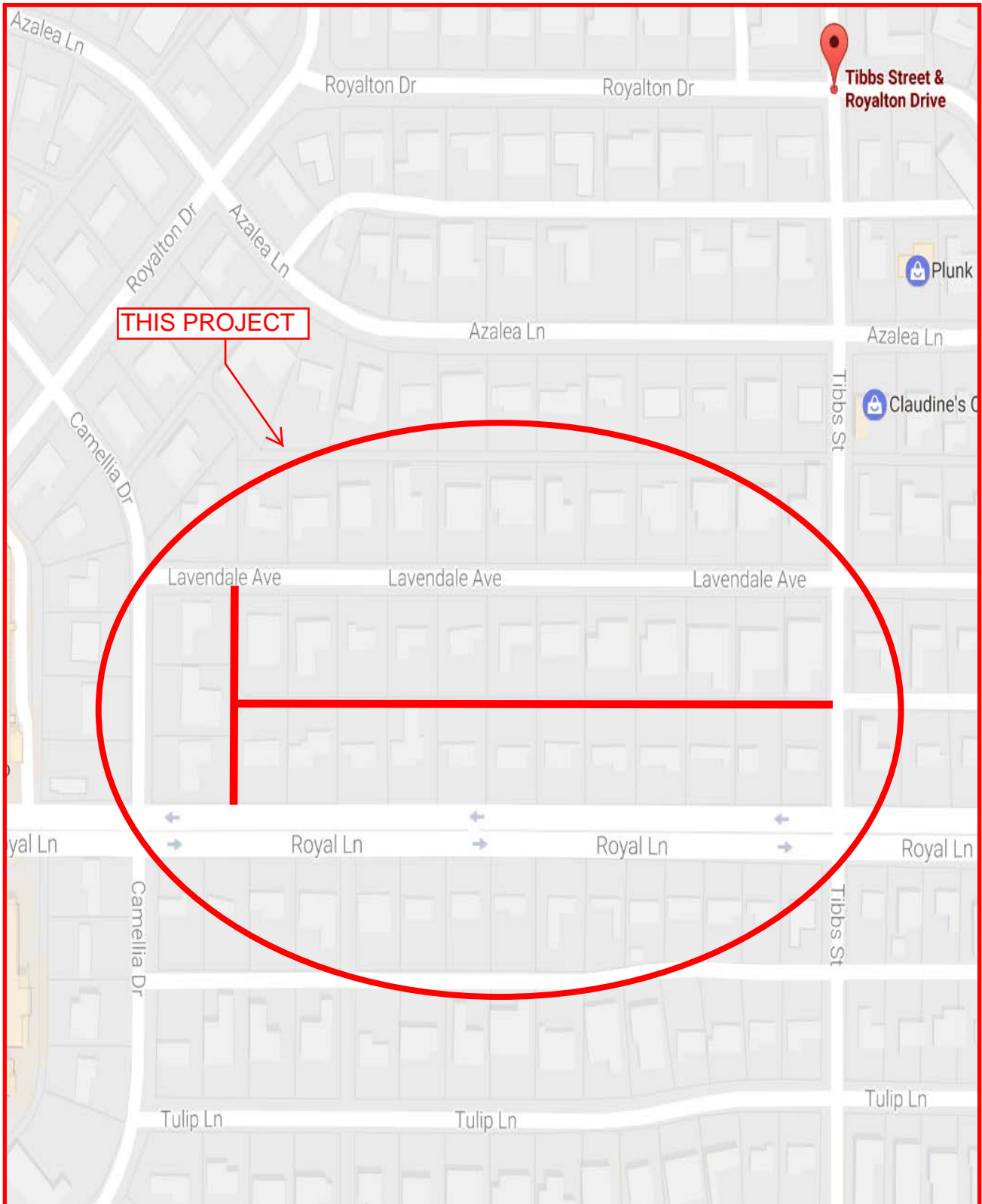
MAPSCO 25H

**ALLEY RECONSTRUCTION GROUP 12-2040
ALLEY BETWEEN LAVENDALE AVENUE AND ROYAL LANE
FROM TIBBS STREET TO EDGEMERE ROAD**



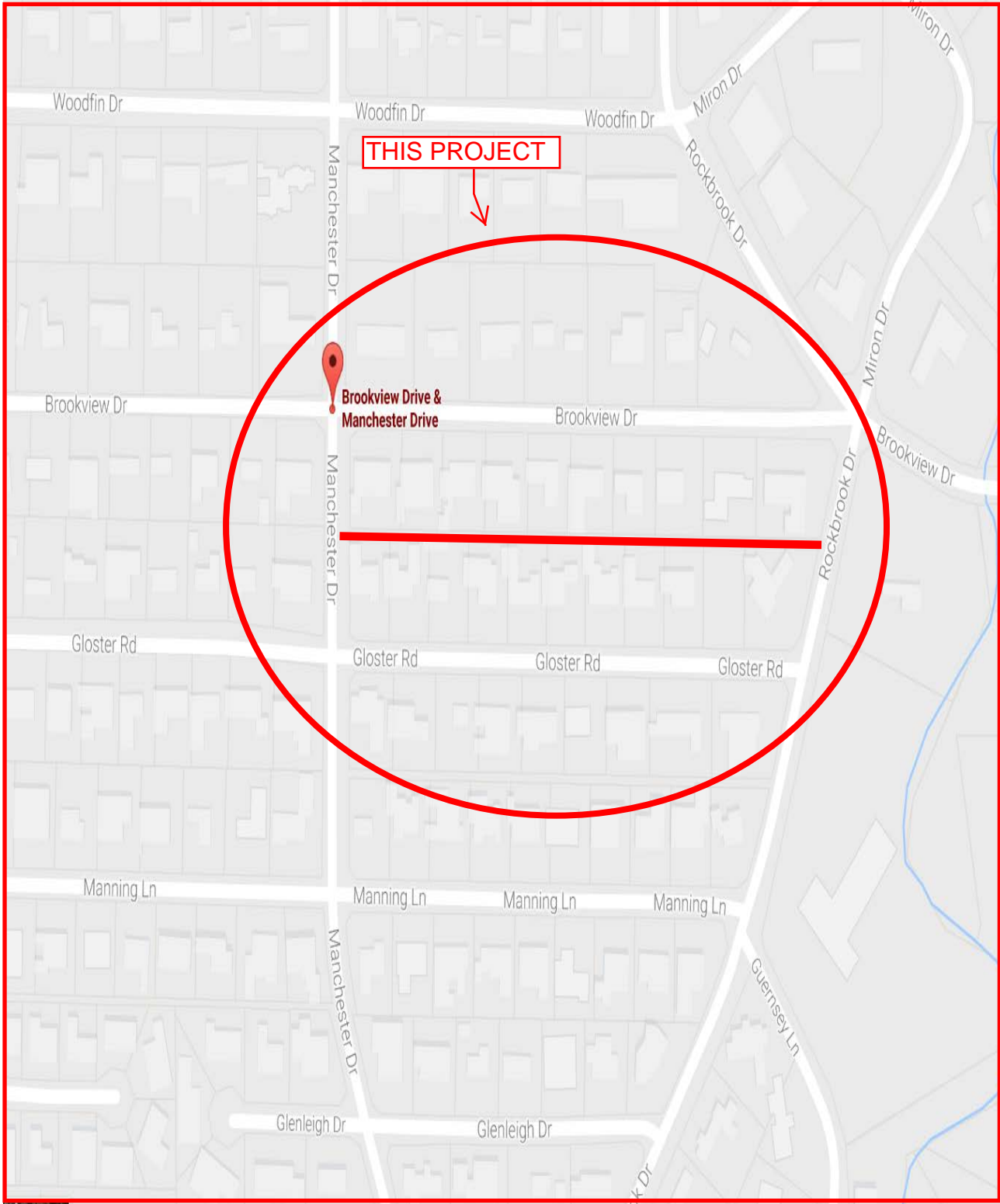
MAPSCO 25G

**ALLEY RECONSTRUCTION GROUP 12-2040
ALLEY BETWEEN LAVENDALE AVENUE AND ROYAL LANE
FROM ROYAL LANE TO TIBBS STREET**



MAPSCO 25F, 25G

**ALLEY RECONSTRUCTION GROUP 12-2045
ALLEY BETWEEN BROOKVIEW DRIVE AND GLOSTER ROAD
FROM MANCHESTER DRIVE TO ROCKBROOK DRIVE**

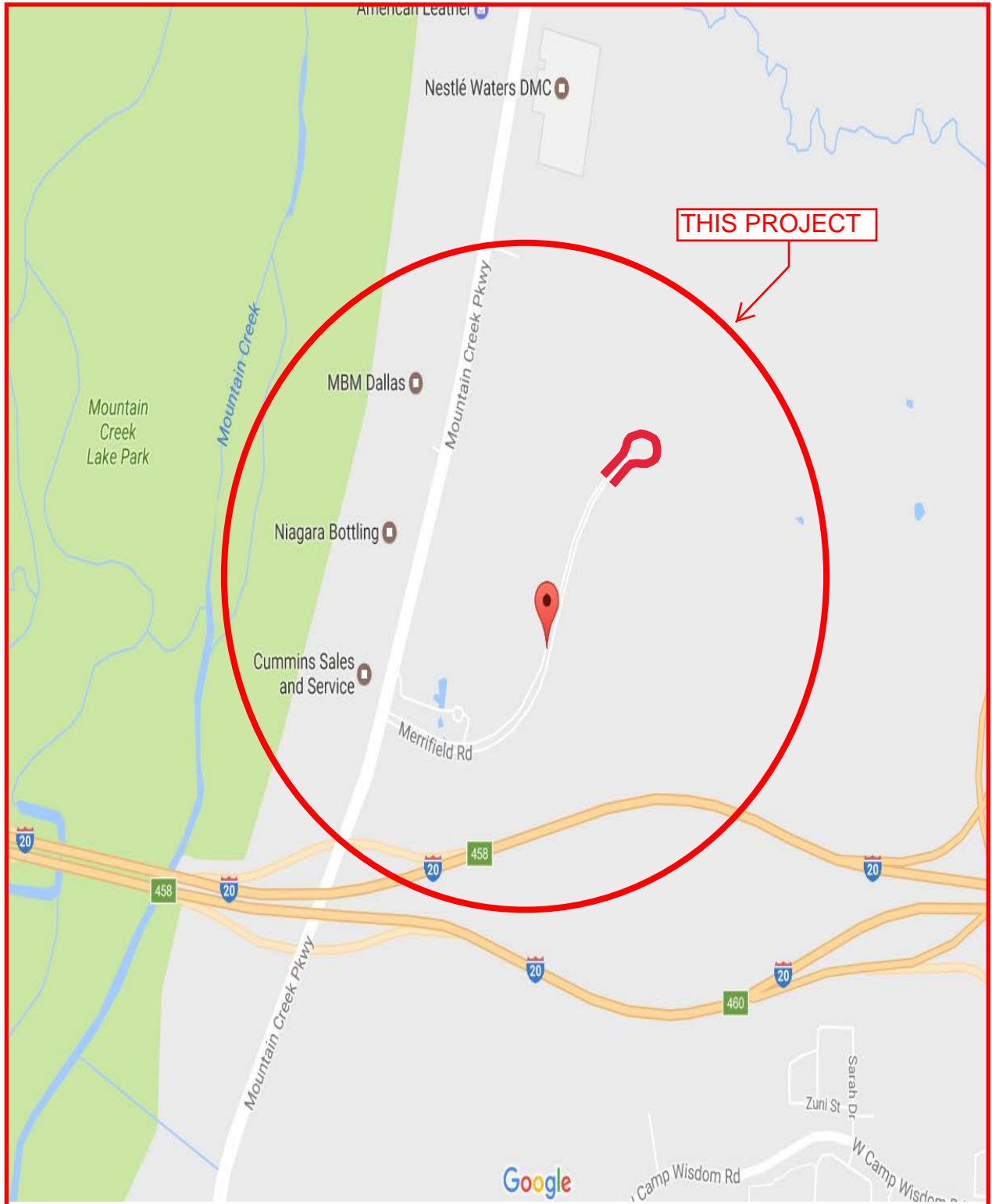


**ALLEY RECONSTRUCTION GROUP 12-2045
ALLEY BETWEEN CARUTH BOULEVARD AND COLGATE AVENUE
FROM DURHAM STREET TO BOEDEKER STREET**



MAPSCO 26W

PAVING IMPROVEMENTS MERRIFIELD ROAD DEAD END TURNAROUND



MAPSCO 61B-J

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: March 22, 2017

COUNCIL DISTRICT(S): 14

DEPARTMENT: Mobility and Street Services
Park & Recreation

CMO: Jill A. Jordan, P.E., 670-5299
Joey Zapata, 670-3009

MAPSCO: 45L

SUBJECT

A public hearing to receive comments to amend the City of Dallas Central Business District Streets and Vehicular Circulation Plan to delete Live Oak Street from Harwood Street to Pacific Avenue/Saint Paul Street; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City

BACKGROUND

The City of Dallas' Park and Recreation Department has requested an amendment to the City of Dallas Central Business District Streets and Vehicular Circulation Plan to delete a section of Live Oak Street. Live Oak Street is a three-lane one-way southbound roadway. On the east side of the road there is a dedicated parking lane and James W. Aston Park. On the west side of the road there is peak hour restricted parking with meters and a parking lot.

This proposed amendment change would facilitate the integration of James W. Aston Park into a larger contiguous open space, Pacific Plaza. City Council adopted the Downtown Parks Master Plan Update in April 2013, which designated Pacific Plaza Park as one of the park priorities. This proposed project provides enhanced open space and will provide a more walkable environment in and around the proposed park.

The proposed amendment would require vehicles to reroute to their destination. There are various options for drivers heading westbound on Live Oak Street to continue west after the closure: one alternative is to turn southbound on Pearl Street to Elm Street to continue farther westbound; another is to head southbound on Harwood Street to Elm Street to continue farther westbound. Both options have been studied in a traffic impact analysis and determined to be feasible.

BACKGROUND (Continued)

Staff recommends approval of the amendment to delete Live Oak Street from Harwood Street to Pacific Avenue/Saint Paul Street from the Central Business District Streets and Vehicular Circulation Plan.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

The City Plan Commission Transportation Committee acted on this item on February 2, 2017, and followed staff recommendation of approval.

The City Plan Commission acted on this item on February 16, 2017, and followed staff recommendation of approval.

Information about this item will be provided to the Transportation and Trinity River Project Committee on February 27, 2017.

MAP

Attached

Live Oak Street

Harwood Street to Pacific Avenue/Saint Paul Street

Council District: 14

MAPSCO: 45L



Central Business District Streets and
Vehicular Circulation Plan Amendment Map



Sources: Esri, HERE, DeLorme, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), MapmyIndia, NGCC, © OpenStreetMap contributors, and the GIS User Community

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: March 22, 2017
COUNCIL DISTRICT(S): 2, 14
DEPARTMENT: Mobility and Street Services
CMO: Jill A. Jordan, P.E., 670-5299
Joey Zapata, 670-3009
MAPSCO: 45L Q R

SUBJECT

A public hearing to receive comments to amend the City of Dallas Central Business District Streets and Vehicular Circulation Plan to reduce the right-of-way on **(1)** Harwood Street between Commerce Street and Jackson Street from 85 feet of right-of-way to 64 feet of right-of-way; and **(2)** Harwood Street between Jackson Street and IH-30 from 78-130 feet of right-of-way to 64-68 feet of right-of-way; and at the close of the hearing, authorize an ordinance implementing the change - Financing: No cost consideration to the City

BACKGROUND

The City has received applications from Viewtech and the Office of Economic Development to amend the Central Business District Streets and Vehicular Circulation Plan. The amendments will modify the sidewalk width on Harwood Street. The requests for reductions in the right-of-way will maintain a 10-foot sidewalk on both sides of the roadway for the pedestrian zone, but will not impact vehicle capacity.

The amendment to Harwood Street between Commerce Street and Jackson Street was requested by Viewtech to reduce the right-of-way to facilitate development of an underground parking garage for the Statler Hotel on land that currently exists as a surface-level parking lot. The surface parking lot encroaches in the City's designated right-of-way; however, the existing sidewalk width is 5.5 feet. The applicant has agreed to dedicate the additional 4.5 feet of parkway to ensure a 10-foot sidewalk adjacent to their development.

BACKGROUND (Continued)

The amendment to Harwood Street between Jackson Street and IH-30 was submitted by the Office of Economic Development. They have requested a reduction to the right-of-way to facilitate development in the Farmers Market area. The Office of Economic Development has requested that the developer of Harvest Lofts add stoops/doors facing Harwood Street. The reduction of the right-of-way is needed to add the requested stoops.

The proposed right-of-way reductions do not preclude the proposed changes being analyzed in the 360 update. Harwood from Pacific Avenue to IH-30 is one of the priority corridors that will be studied for multimodal amenities in more detail in the 360 Plan for conceptual design changes.

Staff recommends approval of the amendment to reduce the right-of-way on (1) Harwood Street between Commerce Street and Jackson Street from 85 feet of right-of-way to 64 feet of right-of-way and (2) Harwood Street between Jackson Street and IH-30 from 78-130 feet of right-of-way to 64-68 feet of right-of-way.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

The City Plan Commission Transportation Committee acted on this item on February 2, 2017, and followed staff recommendation of approval.

The City Plan Commission acted on this item on February 16, 2017, and followed staff recommendation of approval.

Information about this item will be provided to the Transportation and Trinity River Project Committee on February 27, 2017.

MAP

Attached

Harwood Street

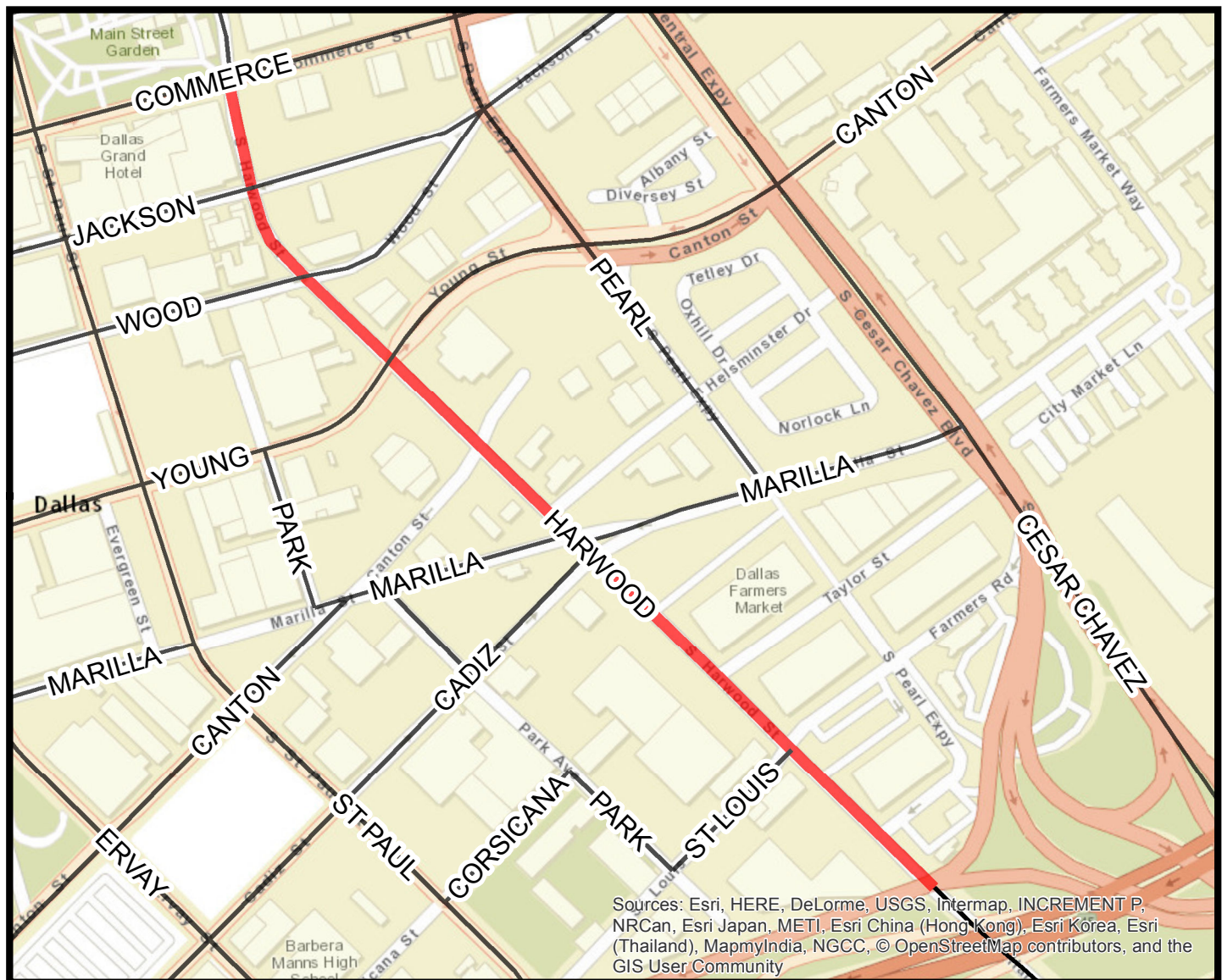
Commerce Street to IH-30

Council District: 2 & 14

MAPSCO: 45L,Q,R



Central Business District Streets and
Vehicular Circulation Amendment Map



KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: March 22, 2017
COUNCIL DISTRICT(S): 1, 3, 4, 9, 10
DEPARTMENT: Trinity Watershed Management
CMO: Mark McDaniel, 670-3256
MAPSCO: 27F K 36R 44Y 63D G U 64F N

SUBJECT

Authorize **(1)** the rejection of bids for Erosion Control Improvements Group 10 and **(2)** the re-advertisement for new bids. – Financing: No cost consideration to the City

BACKGROUND

This project is for the construction of erosion control improvement projects that include the installation of gabion walls, retaining walls, gabion mattresses, slope and channel improvements to protect structures and minimize creek bank erosion along various creeks throughout the city. These projects were funded in the 2012 Bond Program.

On January 20, 2017, four bids were received for the project including Austin Filter Systems, Inc. who was the apparent low bidder.

This action authorizes the rejection of the bids for Erosion Control Group 10 due to low bidder's total amount exceeding the available funding.

This project will be combined with other erosion control projects and provide improved pricing through the advantage of economy of scale.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

No prior action on this.

FISCAL INFORMATION

No cost consideration to the City.

MAP

Attached

March 22, 2017

WHEREAS, bids were received on January 20, 2017 for the Erosion Control Improvement Group 10; and

WHEREAS, the proposals submitted were significantly higher than the available funding; and

WHEREAS, it has been determined that it is in the best interest of the City of Dallas to reject all bids received and re-advertise for new bids.

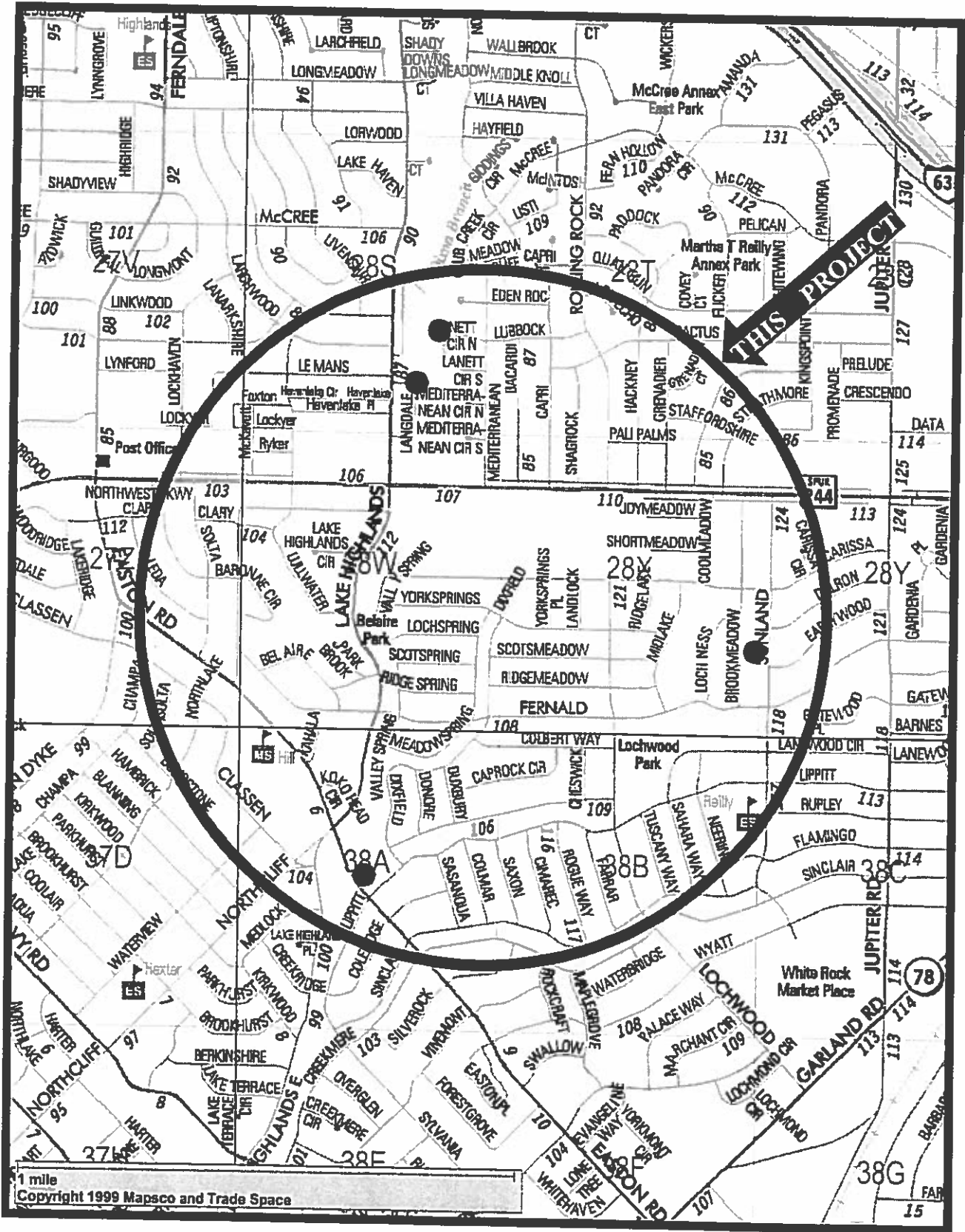
Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to reject the bids received for the Erosion Control Improvements Group 10 and is hereby granted permission to re-advertise.

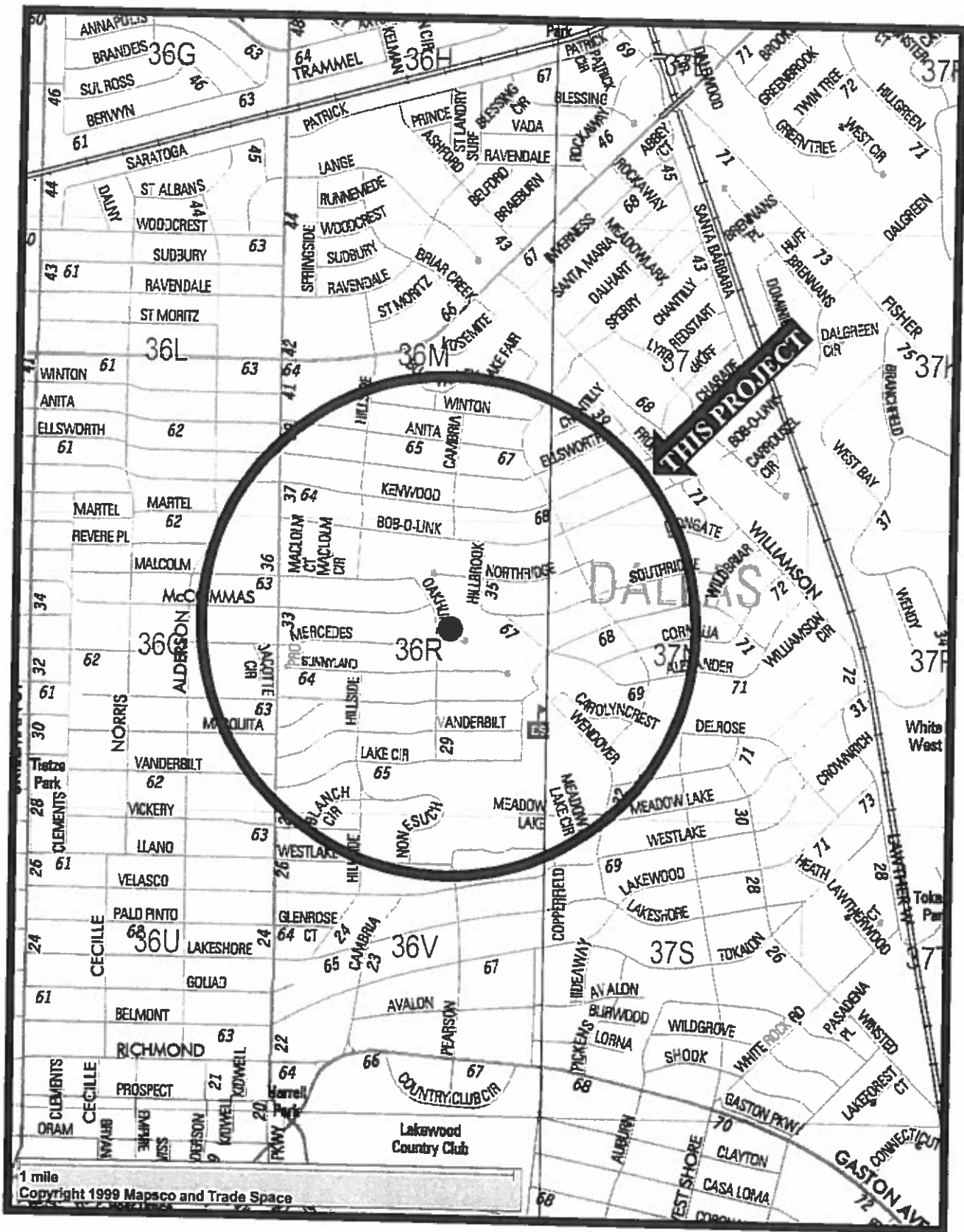
Section 2. That this resolution shall take effective immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

EC Group 6: Map 2 of 4



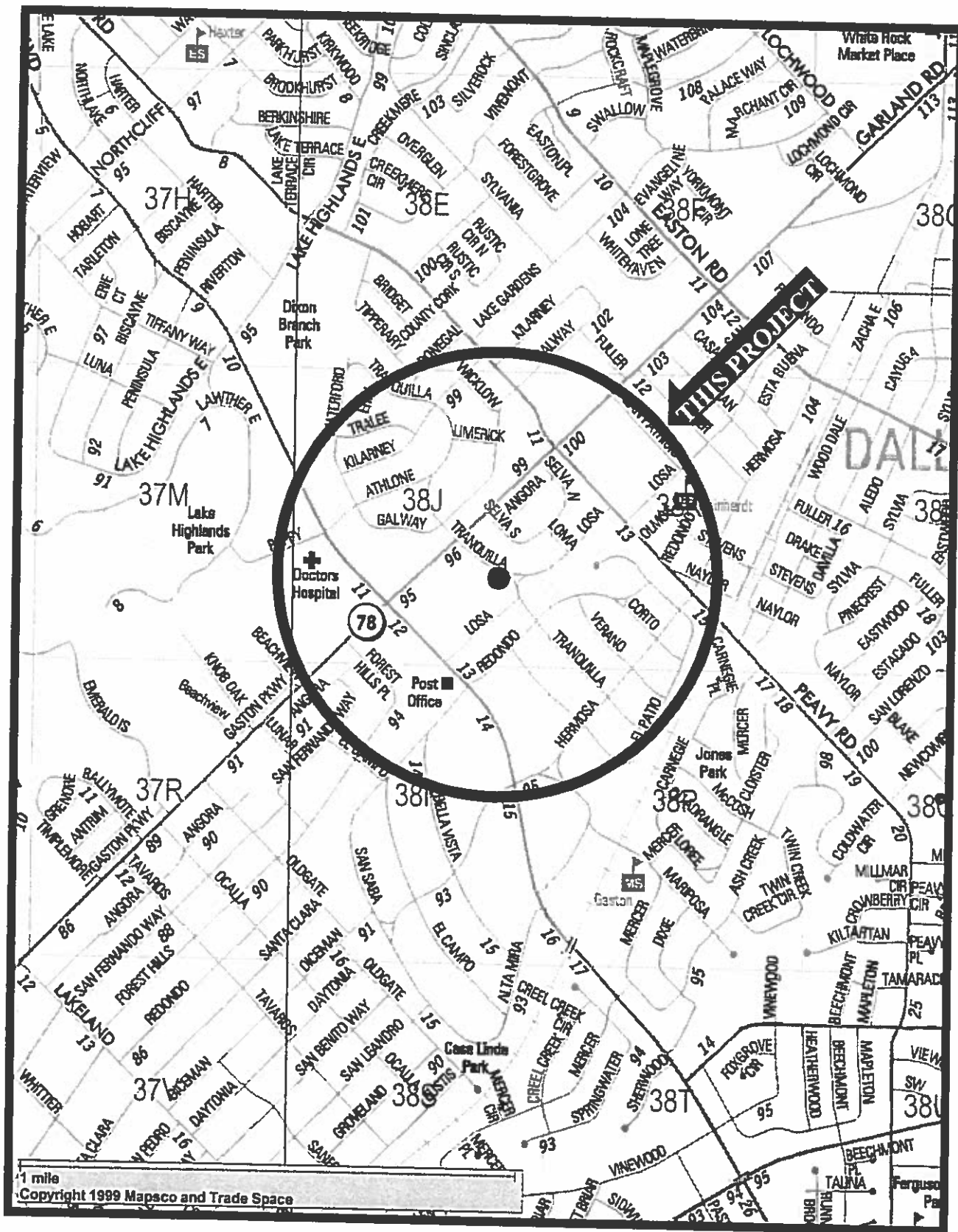
Mapsco 28-S, Y & 38-A

EC Group 6: Map 3 of 4



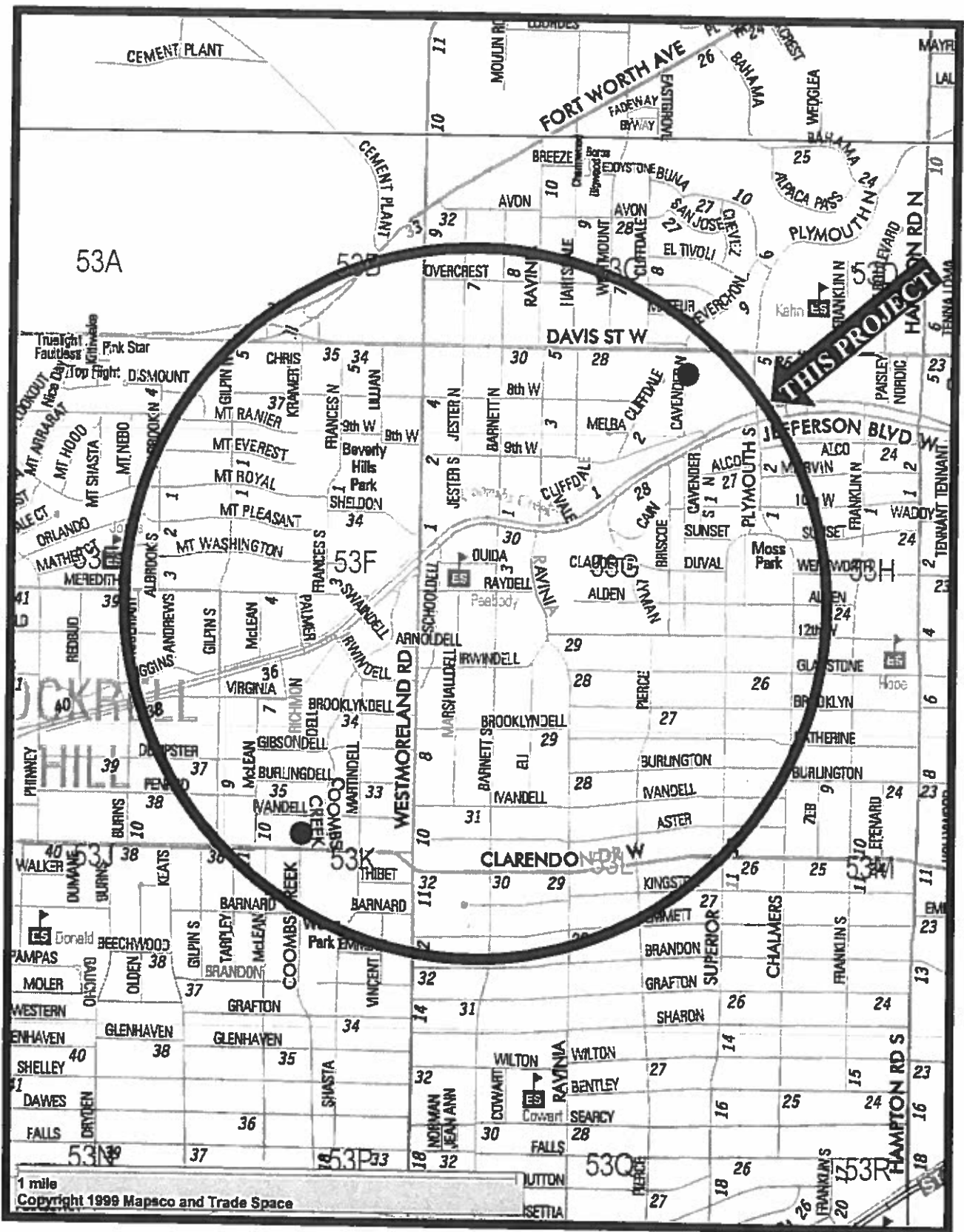
Mapsco 36-R

EC Group 6: Map 4 of 4



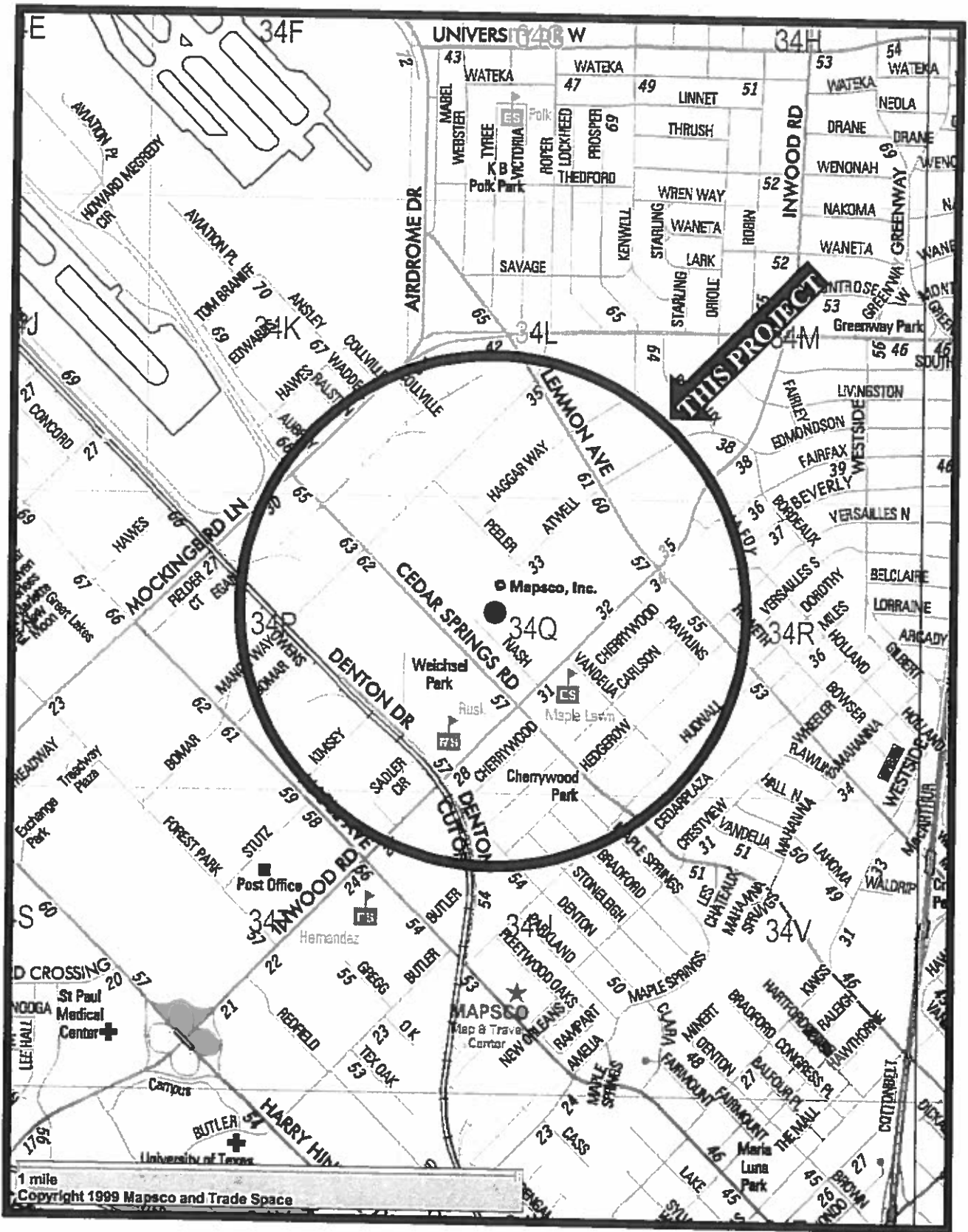
Mapsco 38-J

EC Group 8: Map 1 of 5



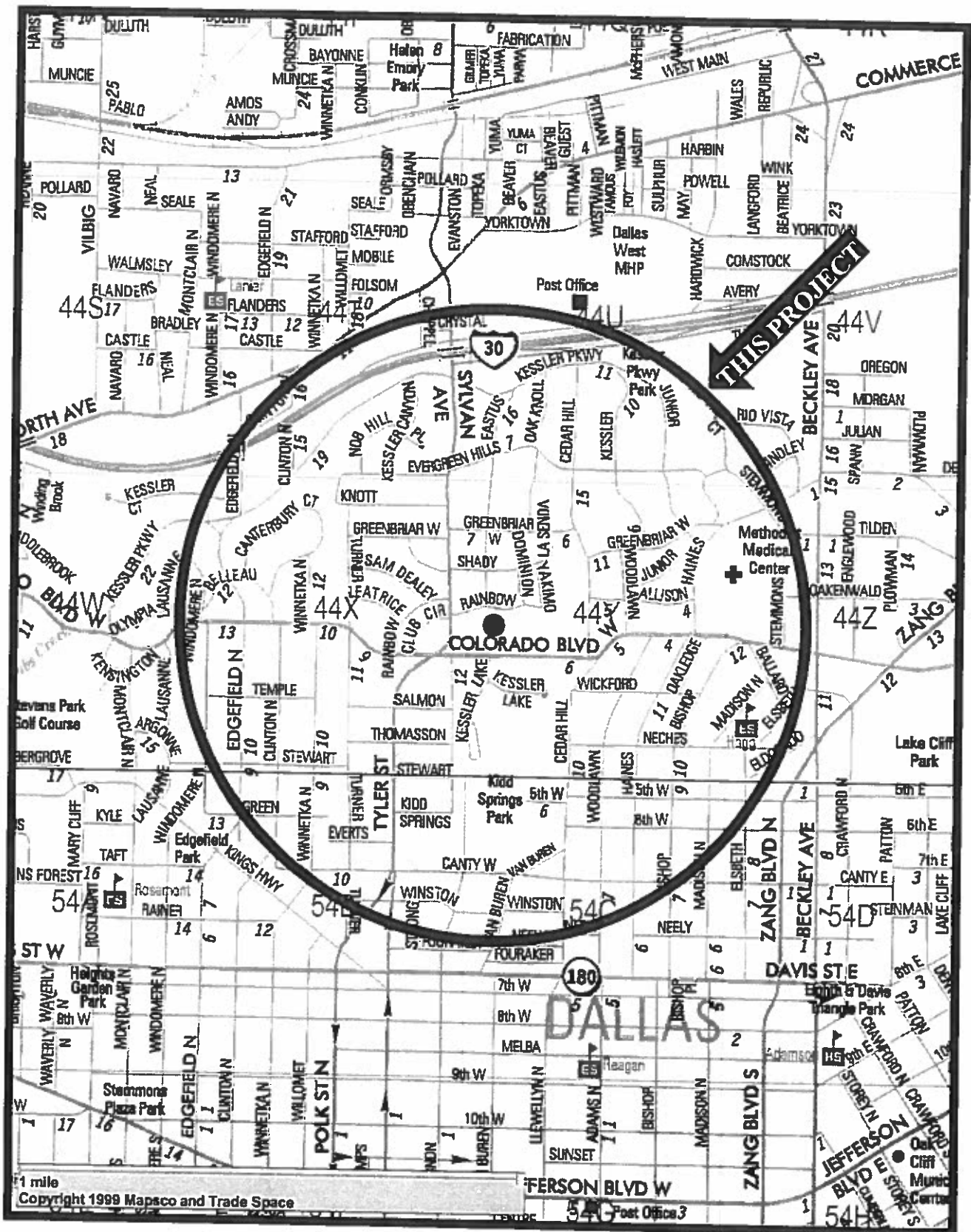
Mapsco 53-C, K

EC Group 8: Map 2 of 5



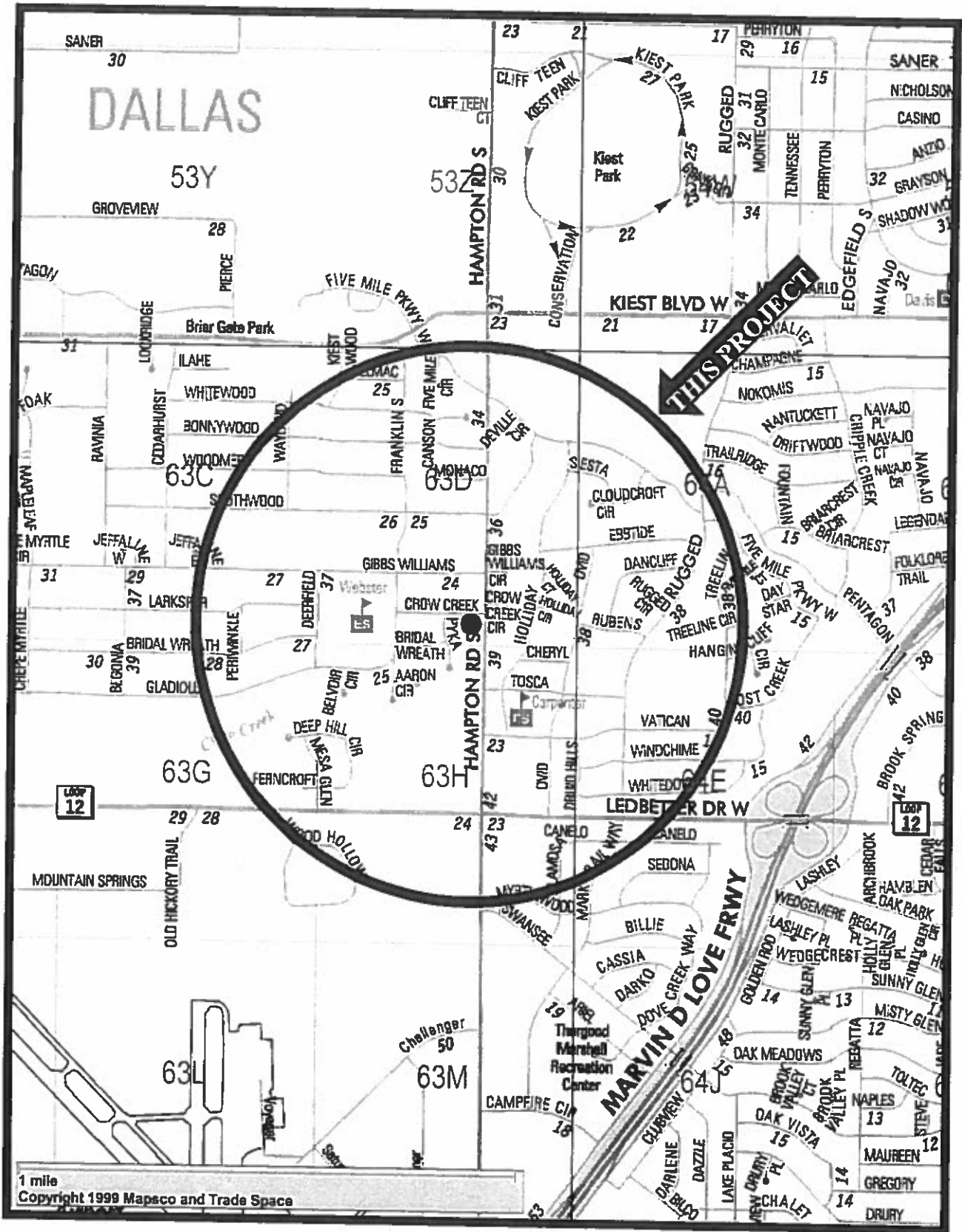
Mapsco 34-Q

EC Group 8: Map 3 of 5



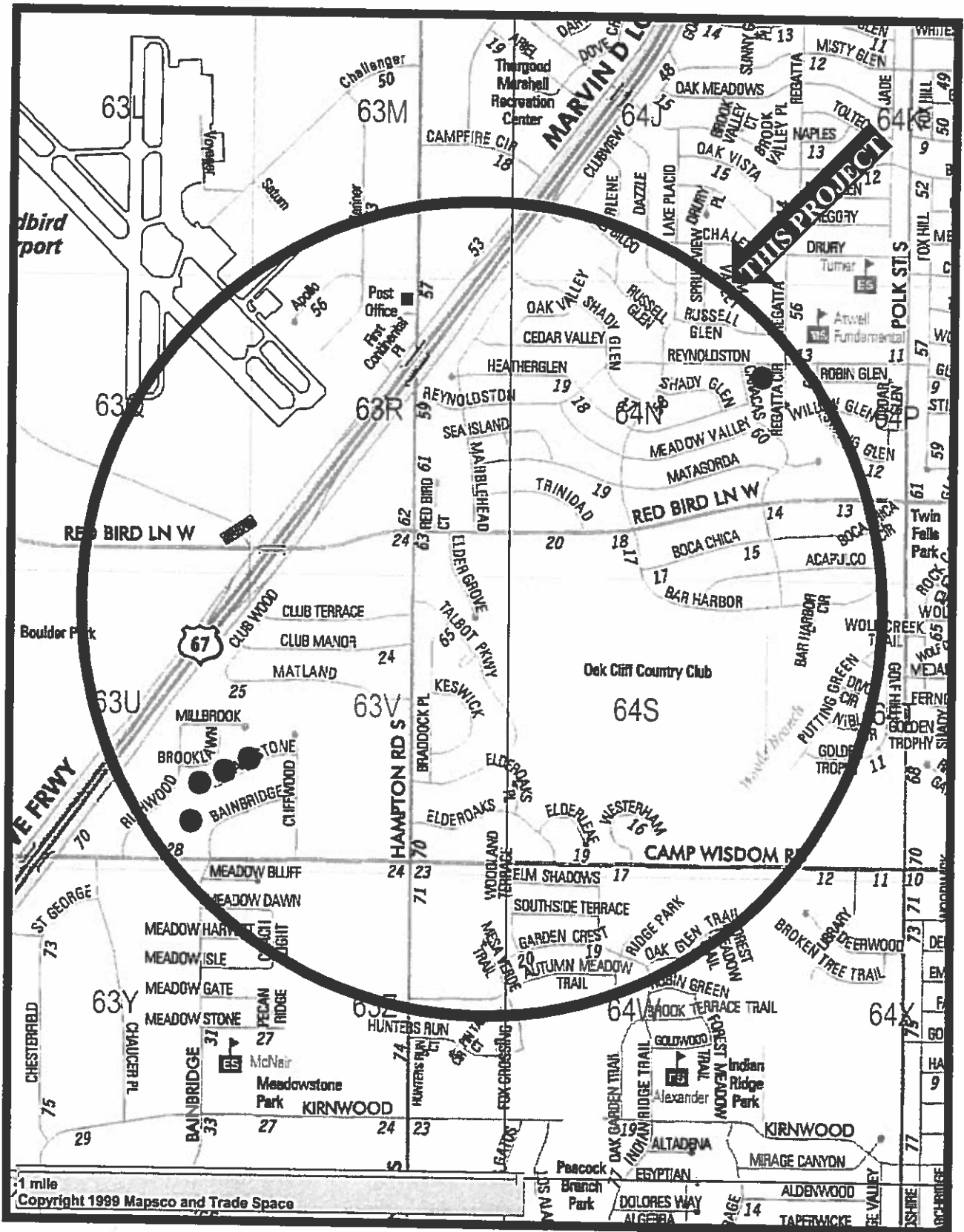
Mapsco 44-Y

EC Group 10: Map 1 of 2



Mapsco 63-D

EC Group 10: Map 2 of 2



Mapsco 63-U & 64-N

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: March 22, 2017
COUNCIL DISTRICT(S): 1. 10, 11
DEPARTMENT: Trinity Watershed Management
CMO: Mark McDaniel, 670-3256
MAPSCO: 15C 16W 26G 27S 53K

SUBJECT

Authorize **(1)** the rejection of bids for Erosion Control Improvements Group 11 and **(2)** the re-advertisement for new bids. – Financing: No cost consideration to the City

BACKGROUND

The Erosion Control Improvements Group 11 comprises the construction of five (5) erosion control improvement projects that include the installation of gabion walls, retaining walls, gabion mattresses, slope and channel improvements to protect structures and minimize creek bank erosion along various creeks throughout the city. These projects were funded in the 2012 Bond Program.

On January 13, 2017, five bids were received for the project including Ark Contracting Services who was the apparent low bidder.

This action authorizes the rejection of the bids for Erosion Control Group 11 because the apparent low bidder's total bid amount exceeded the available funding.

This project will be combined with other erosion control projects and provide improved pricing through the advantage of economy of scale.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

No prior action on this.

FISCAL INFORMATION

No cost consideration to the City.

MAP

Attached

March 22, 2017

WHEREAS, bids were received on January 13, 2017 for the Erosion Control Improvement Group 11; and

WHEREAS, the proposals submitted were significantly higher than the available funding; and

WHEREAS, it has been determined that it is in the best interest of the City of Dallas to reject all bids received and re-advertise for new bids.

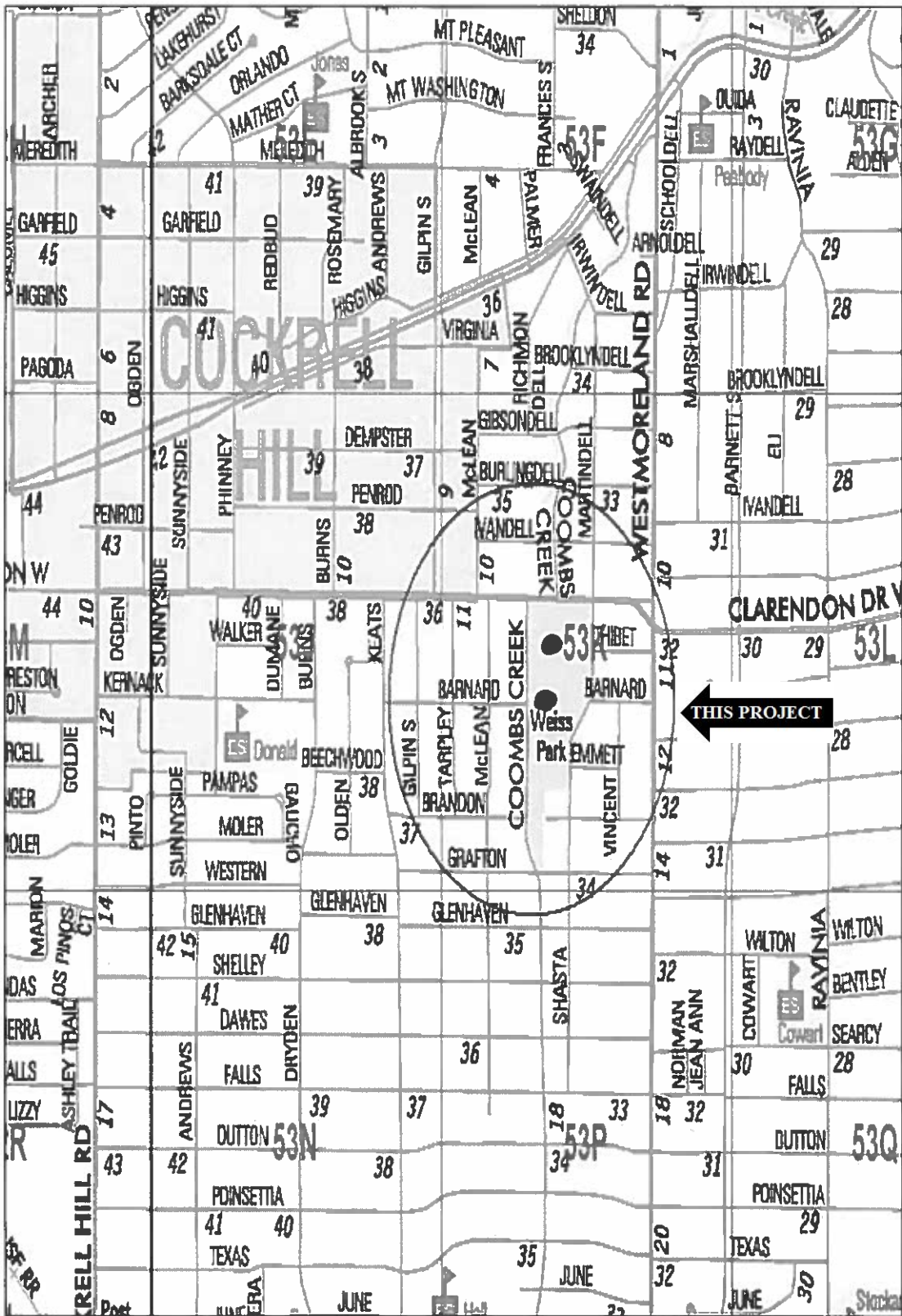
Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to reject the bids received for the Erosion Control Improvements Group 11 and is hereby granted permission to re-advertise.

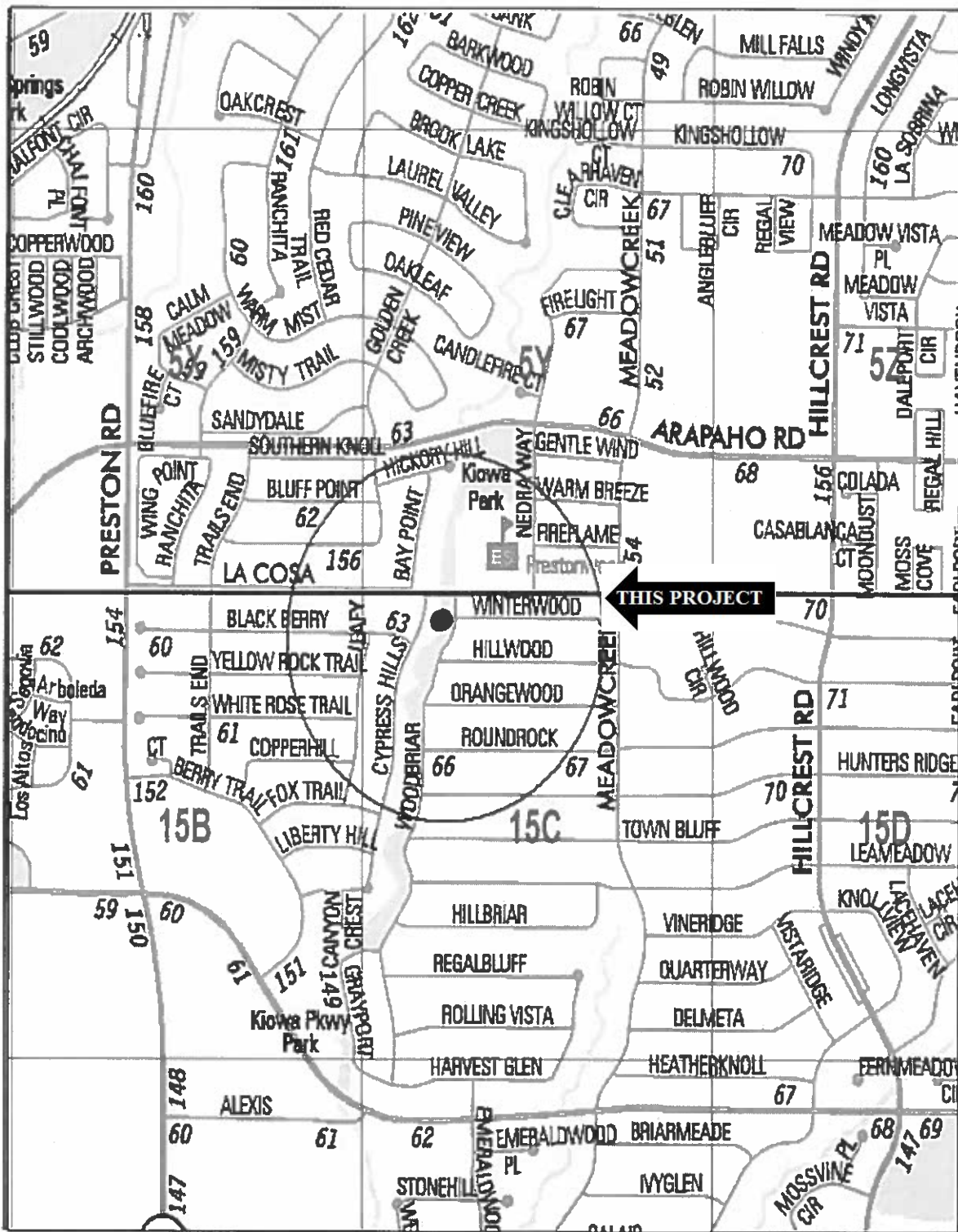
Section 2. That this resolution shall take effective immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

Erosion Control Group 11



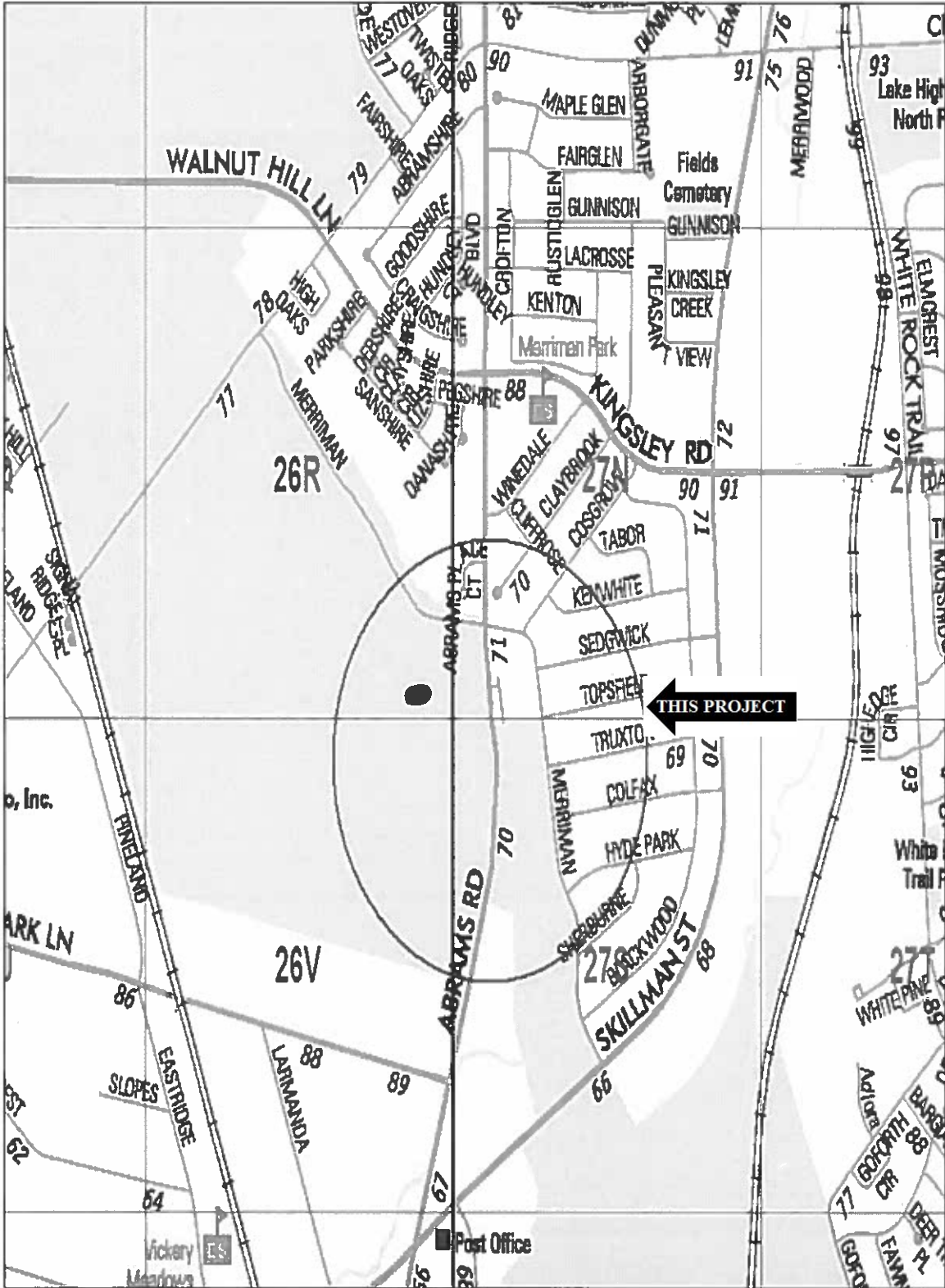
Mapsc0 53-K

Erosion Control Group 11



Mapsco 15-C

Erosion Control Group 11



Mapsc0 27-S

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: March 22, 2017
COUNCIL DISTRICT(S): 1, 2, 4, 6
DEPARTMENT: Trinity Watershed Management
CMO: Mark McDaniel, 670-3256
MAPSCO: 32 D H 43 A B C D E F G H 44 A E F G K L M R V 45 N S W X
Y 55 B C

SUBJECT

Authorize Supplemental Agreement No. 1 to the professional services contract with Halff Associates, Inc. for additional surveying, environmental services and preparation of a Municipal Setting Designation and the Final Plat for the Dallas Floodway - Not to exceed \$179,818, from \$640,498 to \$820,316 - Financing: 1998 Bond Funds (\$52,258) and Stormwater Drainage Management Current Funds (\$127,560)

BACKGROUND

The City of Dallas has been operating and maintaining the levee system since the time it was originally built, and currently owns most of the parcels within the Dallas Floodway. Primary usage of the Dallas Floodway has been water conveyance. Capital improvement plans for the Dallas Floodway include flood risk reduction, ecosystem restoration, transportation and recreation amenities. Completing surveys, platting and environmental services are necessary to facilitate future project development.

Additional professional land surveying services will be required to file the Dallas Floodway Final Plat in fourteen separate phases as requested by the City to support regulatory compliance. A limited area of Volatile Organic Compound (VOC) affected groundwater was identified within a portion of the Dallas Floodway during the initial site investigation activities and this area was entered into the Texas Commission on Environmental Quality (TCEQ) Voluntary Cleanup Program (VCP). The area of VOC affected groundwater requires additional environmental services to develop documentation to obtain a TCEQ VCP Certificate of Completion.

BACKGROUND (Continued)

This action will authorize Half Associates, Inc. to perform additional surveying and environmental services.

ESTIMATED SCHEDULE OF PROJECT

Began MSD Design August 2014
Complete MSD and Plat December 2017

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized a professional services contract with Half Associates, Inc. for surveying, plating, environmental services, and preparation of a municipal setting designation application for the Dallas Floodway on March 26, 2014, by Resolution No. 14-0561.

Information about this item was provided to the Transportation and Trinity River Project Committee on January 23, 2017.

FISCAL INFORMATION

1998 Bond Funds - \$ 52,258.00
Stormwater Drainage Management Current Funds - \$127,560.00

<u>Council District</u>	<u>Amount</u>
1	\$ 26,972.70
2	\$ 26,972.70
4	\$ 17,981.80
6	<u>\$107,890.80</u>
Total	\$179,818.00

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

Half Associates, Inc.

Hispanic Female	24	Hispanic Male	83
African-American Female	8	African-American Male	15
Other Female	8	Other Male	16
White Female	106	White Male	343

OWNER

Half Associates, Inc.

Kent Belaire P.G., Vice President

BUSINESS INCLUSION AND DEVELOPMENT PLAN SUMMARY

PROJECT: Authorize Supplemental Agreement No. 1 to the professional services contract with Halff Associates, Inc. for additional surveying, environmental services and preparation of a Municipal Setting Designation and the Final Plat for the Dallas Floodway - Not to exceed \$179,818, from \$640,498 to \$820,316 - Financing: 1998 Bond Funds (\$52,258) and Stormwater Drainage Management Current Funds (\$127,560)

Halff Associates, Inc. is a local, non-minority firm, has signed the "Business Inclusion & Development" documentation, and proposes to use the following sub-contractors.

PROJECT CATEGORY: Professional Services

LOCAL/NON-LOCAL CONTRACT SUMMARY - THIS ACTION ONLY

	<u>Amount</u>	<u>Percent</u>
Local contracts	\$176,818.00	98.33%
Non-local contracts	\$3,000.00	1.67%
TOTAL THIS ACTION	\$179,818.00	100.00%

LOCAL/NON-LOCAL M/WBE PARTICIPATION THIS ACTION

Local Contractors / Sub-Contractors

<u>Local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Garcia Land Data Inc.	HMMB64109N0217	\$33,931.00	19.19%
Total Minority - Local		\$33,931.00	19.19%

Non-Local Contractors / Sub-Contractors

<u>Non-local</u>	<u>Certification</u>	<u>Amount</u>	<u>Percent</u>
Sunbelt Industrial Services	WFWB64135Y0217	\$3,000.00	100.00%
Total Minority - Non-local		\$3,000.00	100.00%

TOTAL M/WBE PARTICIPATION

	<u>This Action</u>		<u>Participation to Date</u>	
	<u>Amount</u>	<u>Percent</u>	<u>Amount</u>	<u>Percent</u>
African American	\$0.00	0.00%	\$0.00	0.00%
Hispanic American	\$33,931.00	18.87%	\$109,400.00	13.34%
Asian American	\$0.00	0.00%	\$0.00	0.00%
Native American	\$0.00	0.00%	\$0.00	0.00%
WBE	\$3,000.00	1.67%	\$16,500.00	2.01%
Total	\$36,931.00	20.54%	\$125,900.00	15.35%

March 22, 2017

WHEREAS, on March 26, 2014, Resolution No. 14-0561 authorized a professional services contract with Halff Associates, Inc., for surveying, platting, environmental services, and the preparation of a municipal setting designation application of the Dallas Floodway in the amount of \$640,497.75; and,

WHEREAS, it is now necessary to authorize Supplemental Agreement No. 1 to the professional services contract with Halff Associates, Inc., for additional surveying, environmental services and preparation of a Municipal Setting designation and the Final Plat for the Dalals Floodway, in an amount not to exceed \$179,818.00, increasing the contract from \$640,497.75 to \$820,315.75.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to execute Supplemental Agreement No. 1 to the professional services contract with Halff Associates, Inc., for additional surveying, environmental services and preparation of a Municipal Setting designation and the Final Plat for the Dalals Floodway, in an amount not to exceed \$179,818.00, increasing the contract from \$640,497.75 to \$820,315.75, after it has been approved as to form by the City Attorney.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in accordance with the terms and conditions of the contract from:

Trinity River Corridor Project Funds Fund 5P14, Dept. PBW, Unit N963, Act. TRPP Obj. 4114, Program PB98N963, CT PBW98N963F1 Vendor # 089861, in an amount not to exceed	\$52,258.00
Stormwater Drainage Management Current Funds Fund 0061, Dept. SDM, Unit 4792, Act. SD01 Object 3070, Program No. PB98N963, CT SDM4792BM35 Vendor # 089861, in an amount not to exceed	<u>\$127,560.00</u>
Total amount not to exceed	\$179,818.00

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.

KEY FOCUS AREA: Economic Vibrancy
AGENDA DATE: March 22, 2017
COUNCIL DISTRICT(S): 6
DEPARTMENT: Trinity Watershed Management
CMO: Mark McDaniel, 670-3256
MAPSCO: 22S

SUBJECT

A public hearing to receive comments regarding the application for and approval of the fill permit and removal of the floodplain (FP) prefix from approximately 0.788 acres of the current 7.298 acres of land, located at 1717 W. Northwest Hwy, within the floodplain of Elm Fork of the Trinity River, Fill Permit 16-01 - Financing: No cost consideration to the City

BACKGROUND

This request is to fill approximately 0.788 acres of the current 7.298 acres of floodplain, to be used for a future commercial facility. The proposed fill is located at 1717 W. Northwest Highway, within the floodplain of the Elm Fork of Trinity River.

A neighborhood meeting was held at the Bachman Therapeutic Recreation Center on February 13, 2017. Attendees included the developer's engineer, the developer's representative, and *** city staff members. *** citizens from the area attended. There has been no objection to the fill permit.

The fill permit application meets all engineering requirements for filling in the floodplain as specified in Part II of the Dallas Development Code, Section 51A-5.105(h). The applicant has not requested a waiver of any criteria. Accordingly, the City Council should approve this application; or, it may pass a resolution to authorize acquisition of the property under the laws of eminent domain and may then deny the application in order to preserve the status quo until acquisition.

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

This item has no prior action.

FISCAL INFORMATION

No cost consideration to the City.

OWNER/APPLICANT

Mr. Bryan Piper, representing RKM utilities, Inc.
1805 Royal Lane, Suite 107
Dallas, Texas 75229

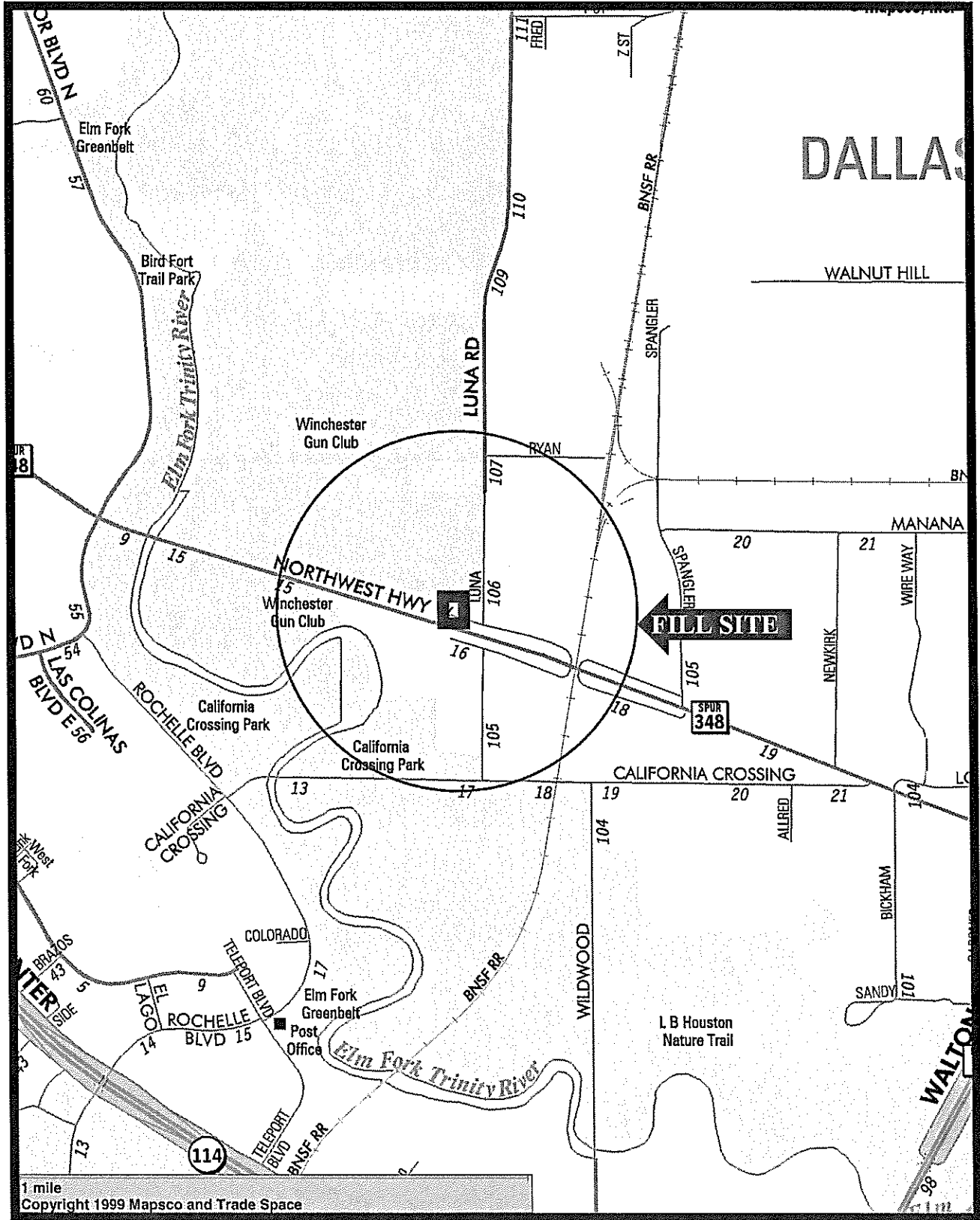
ENGINEER

Mrs. Olivia Whittaker, P.E., CFM
Jacobs Engineering Group Inc.
1999 Bryan Street, Suite 1200
Dallas, Texas 75201

MAP

Attached.

FILL PERMIT 15-06



MAPSCO 22-S

KEY FOCUS AREA: Economic Vibrancy

AGENDA DATE: March 22, 2017

COUNCIL DISTRICT(S): 9, 10

DEPARTMENT: Mobility and Street Services
Water Utilities

CMO: Jill A. Jordan, P.E., 670-5299
Mark McDaniel, 670-3256

MAPSCO: 27T U 38L

SUBJECT

Authorize a construction contract with Jeske Construction Company, lowest responsive bidder of five, for the construction of street and alley paving, storm drainage, water and wastewater main improvements for Project Group 12-3004 (list attached) - Not to exceed \$911,793 - Financing: 2012 Bond Funds (\$729,680), Water Utilities Capital Improvement Funds (\$168,613) and Water Utilities Capital Construction Funds (\$13,500)

BACKGROUND

On January 22, 2014, Resolution No. 14-0190 authorized a professional services contract with BDS Technologies, Inc. for the engineering design of street and alley improvements. This action will authorize a construction contract with Jeske Construction Company for the construction of street and alley paving, storm drainage, water and wastewater main improvements for Project Group 12-3004, which consists of street improvement for Stevens Street from Cayuga Drive to Davilla Avenue and Davilla Avenue from Stevens Street to Drake Street, and the alley southwest and parallel to Shoreview Road from Forest Trail to Eagle Trail to Nimrod Trail. The street improvements will consist of upgrading the existing unimproved two-lane asphalt street with 26-foot wide concrete pavement with curbs, sidewalks, drive approaches, drainage installation system, water and wastewater main improvements. The alley improvements will consist of upgrading the unimproved gravel and dirt alley with 10-foot wide concrete pavement.

BACKGROUND (Continued)

The following chart shows Jeske Construction Company's completed contractual activities for the past three years.

	<u>STS</u>	<u>WTR</u>	<u>PKR</u>
Projects Completed	8	0	0
Change Orders	0	0	0
Projects Requiring Liquidated Damages	0	0	0
Projects Completed by Bonding Company	0	0	0

ESTIMATED SCHEDULE OF PROJECT

Began Design	March 2014
Completed Design	October 2016
Begin Construction	May 2017
Complete Construction	May 2018

PRIOR ACTION/REVIEW (COUNCIL, BOARDS, COMMISSIONS)

Authorized a professional services contract for engineering services on January 22, 2014, by Resolution No. 14-0190.

Authorized street paving improvements and a benefit assessment hearing on January 11, 2017, by Resolution No. 17-0081.

This item was considered by the City Council at a public hearing on February 22, 2017, and was deferred by Councilmember Clayton until March 22, 2017, with the public hearing open.

Information about this item was provided to the Transportation and Trinity River Project Committee on February 27, 2017.

FISCAL INFORMATION

2012 Bond Funds - \$729,680.00
Water Utilities Capital Construction Funds - \$13,500.00
Water Utilities Capital Improvement Funds - \$168,613.00

Design	\$ 61,069.75
Construction (this action)	
Paving & Drainage - STS	\$729,680.00
Water & Wastewater - DWU	<u>\$182,113.00</u>
Total Project Cost	\$972,862.75

<u>Council District</u>	<u>Amount</u>
9	\$590,068.50
10	<u>\$321,724.50</u>
Total	\$911,793.00

This project does involve assessments.

M/WBE INFORMATION

See attached.

ETHNIC COMPOSITION

Jeske Construction Company

Hispanic Female	0	Hispanic Male	18
African-American Female	0	African-American Male	8
Other Female	0	Other Male	0
White Female	1	White Male	5

BID INFORMATION

The following bids with quotes were received and opened on December 2, 2016.

*Denotes successful bidder

<u>BIDDERS</u>	<u>BID AMOUNT</u>
*Jeske Construction Company P. O. Box 59025 Dallas, TX 75229	\$ 911,793.00
Axis Contracting, Inc.	\$1,005,144.00
Camino Construction, LP	\$1,024,979.50
ARK Contracting Services	\$1,107,892.00
**MACVAL Associates, LLC	Non-responsive

**MACVAL Associates, LLC was deemed non-responsive due to not meeting specifications.

Original estimate: STS - \$733,805.00
WTR - \$152,923.76

Total \$886,728.76

OWNER

Jeske Construction Company

Steve Jeske, President

MAPS

Attached.

Project Group 12-3004

<u>Project</u>	<u>Limits</u>	<u>Council District</u>
Stevens Street	from Cayuga Drive to Davilla Avenue and Davilla Avenue from Stevens Street to Drake Street	9
Alley southwest and parallel to Shoreview Road	from Forest Trail to Eagle Trail to Nimrod Trail	10

March 22, 2017

WHEREAS, on January 22, 2014, Resolution No. 14-0190 authorized a professional services contract with BDS Technologies, Inc. for the engineering design services for Project Group 12-3004, in the amount of \$284,658.75; and,

WHEREAS, on January 11, 2017, Resolution No. 17-0081 authorized improvements and the public hearing; and,

WHEREAS, bids were received on December 2, 2016, for the construction of street and alley paving, storm drainage, water and wastewater main improvements for Project Group 12-3004, as follows:

<u>BIDDERS</u>	<u>BID AMOUNT</u>
Jeske Construction Company	\$ 911,793.00
Axis Contracting, Inc.	\$1,005,144.00
Camino Construction, LP	\$1,024,979.50
ARK Contracting Services	\$1,107,892.00
**MACVAL Associates, LLC	Non-responsive

**MACVAL Associates, LLC was deemed non-responsive due to not meeting specifications.

Now, Therefore,

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF DALLAS:

Section 1. That the City Manager is hereby authorized to execute a construction contract with Jeske Construction Company for the construction of street and alley paving, storm drainage and water and wastewater main improvements for Project Group 12-3004, in an amount not to exceed \$911,793, this being the second lowest responsive bid received as indicated by the tabulation of bids, after it has been approved as to form by the City Attorney.

Section 2. That the Chief Financial Officer is hereby authorized to disburse funds in accordance with the terms and conditions of the contract from:

Street and Transportation Improvements Fund	
Fund 3U22, Department STS, Unit S208, Act. APET	
Obj. 4510, Program #PB12S208, CT STS12S208E1	
Vendor # 083791, in an amount not to exceed	\$254,600.00

March 22, 2017

Street and Transportation Improvements Fund Fund 3U22, Department STS, Unit S426, Act. STPT Obj. 4510, Program # PB12S426, CT STS12S208E1 Vendor # 083791, in an amount not to exceed	\$475,080.00
Water Construction Fund Fund 0102, Department DWU, Unit CW42, Obj. 3221 Program #717009X, CT-PBW717009EN Vendor #083791, in an amount not to exceed	\$ 9,660.00
Water Capital Improvement Fund Fund 3115, Department DWU, Unit PW42, Obj. 4550 Program #717009, CT-PBW717009CP Vendor #083791, in an amount not to exceed	\$105,588.00
Wastewater Construction Fund Fund 0103, Department DWU, Unit CS42, Obj. 3222 Program #717010X, CT-PBW717010EN Vendor #083791, in an amount not to exceed	\$ 3,840.00
Wastewater Capital Improvement Fund Fund 3116, Department DWU, Unit PS42, Obj. 4560 Program #717010, CT-PBW717010CP Vendor #083791, in an amount not to exceed	<u>\$ 63,025.00</u>
Total in an amount not to exceed	\$911,793.00

Section 3. That this resolution shall take effect immediately from and after its passage in accordance with the provisions of the Charter of the City of Dallas, and it is accordingly so resolved.